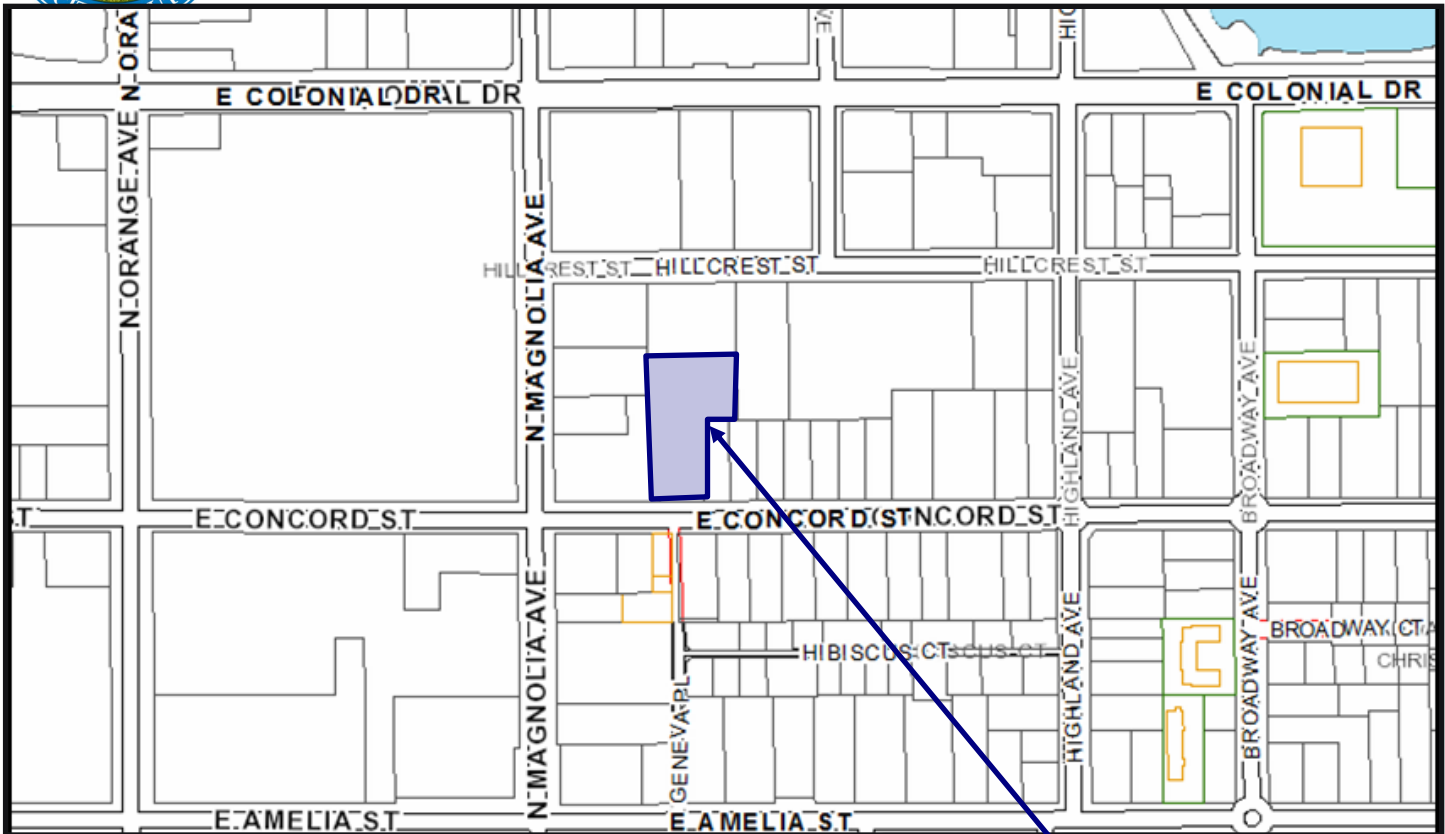




LAKE EOLA HEIGHTS TOWNHOMES PD



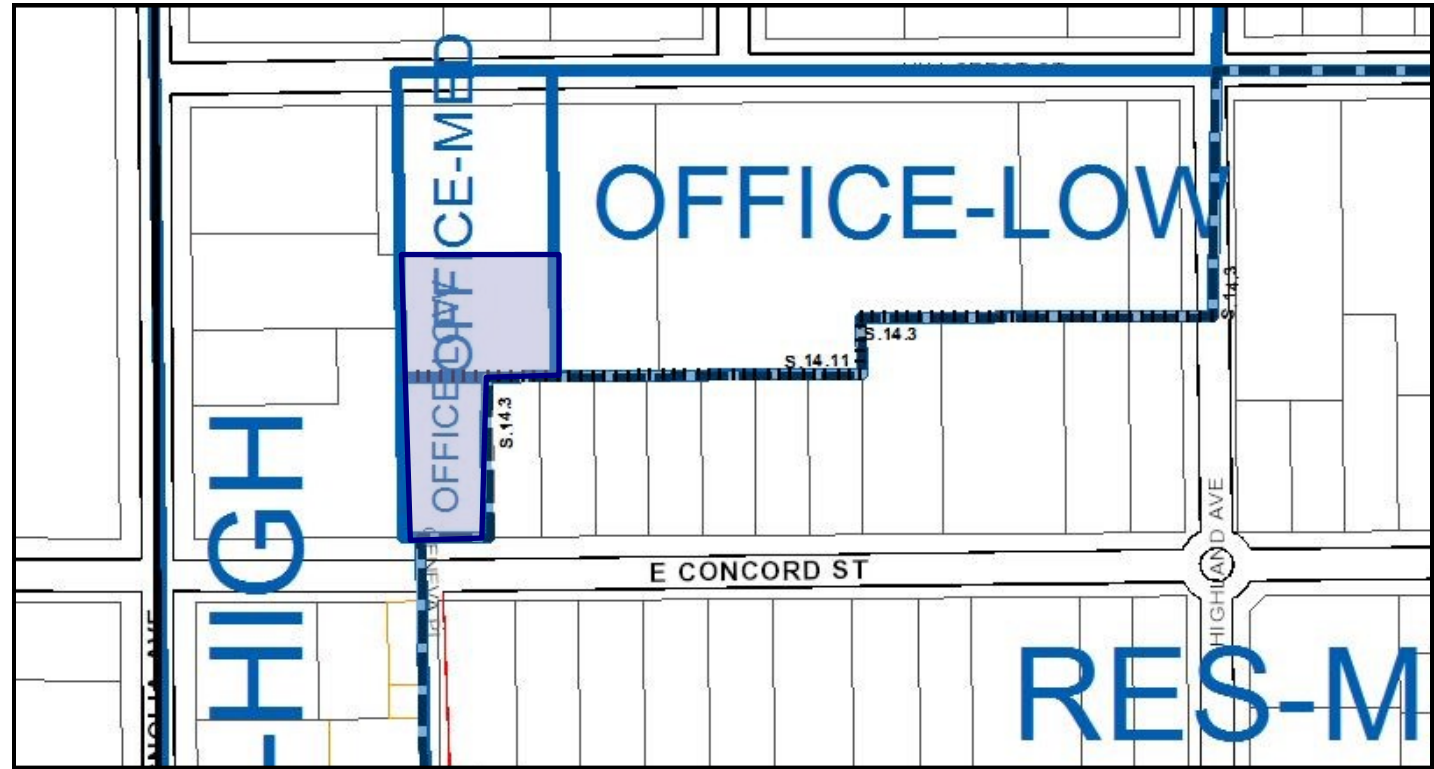
Location Map

Subject Site

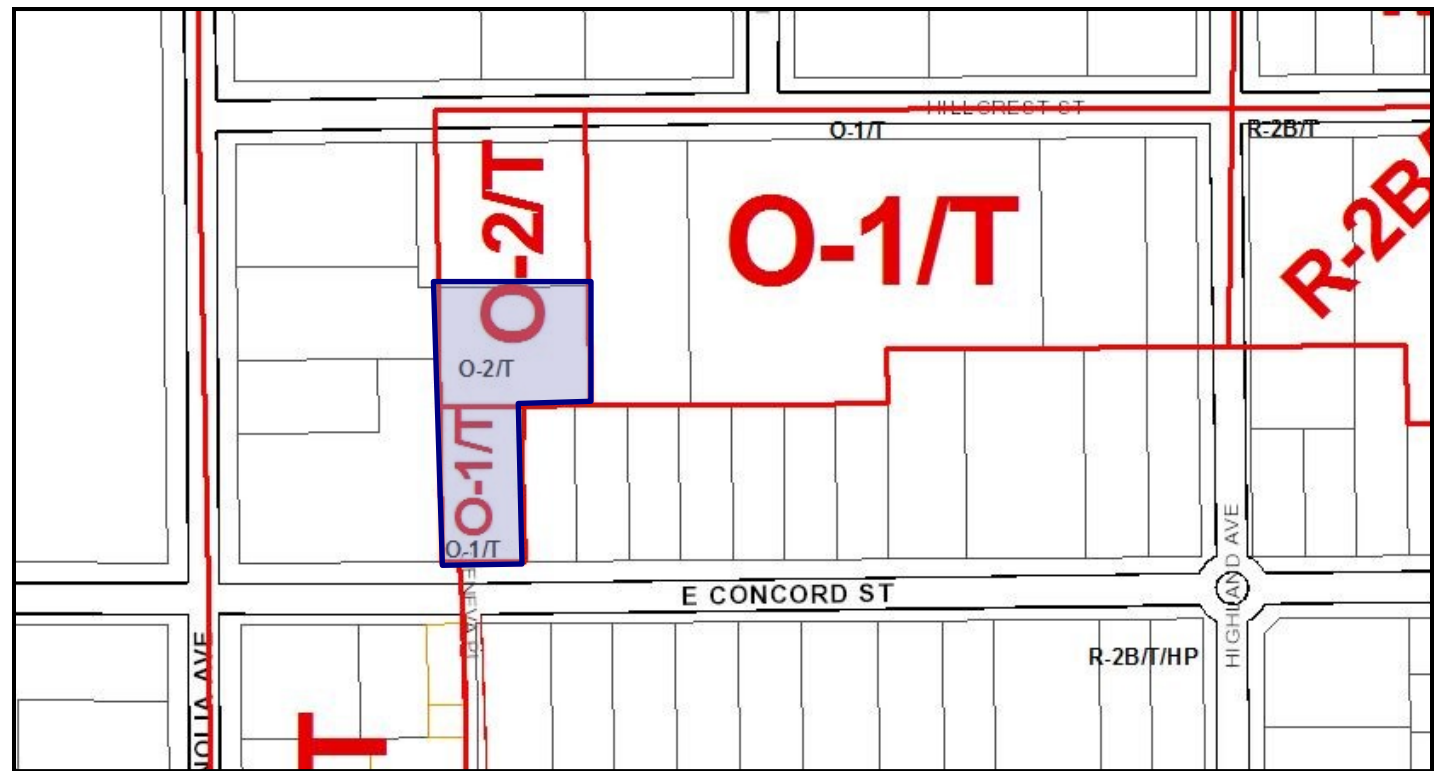
SUMMARY

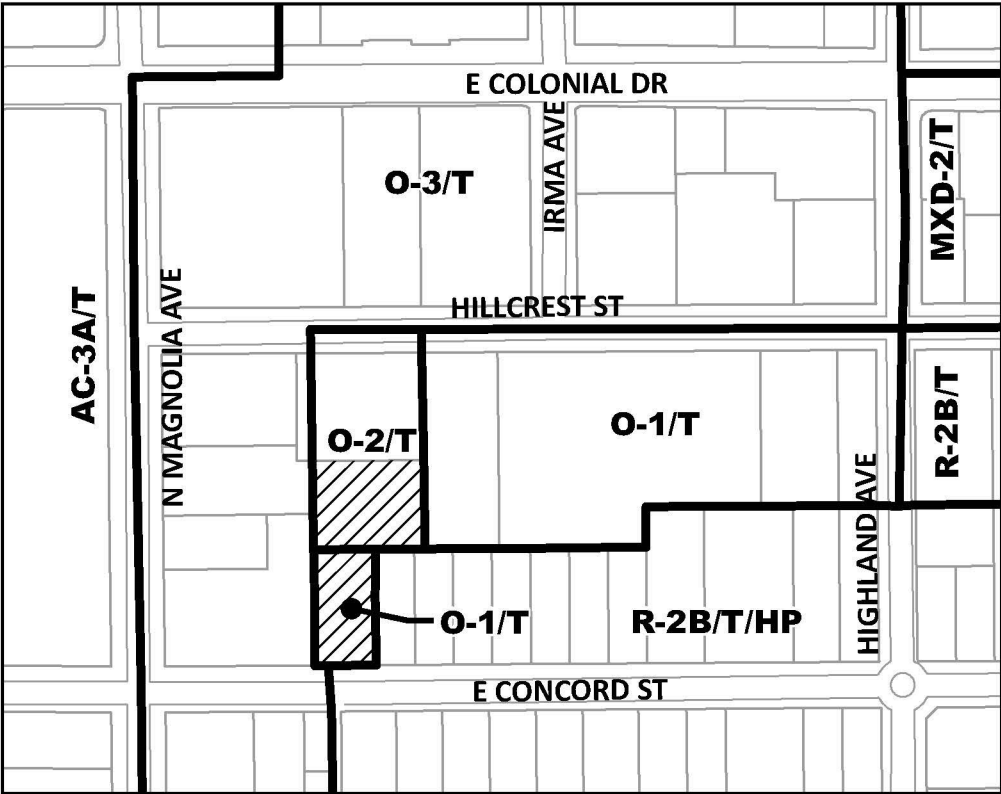
<p>Owner Ray Design Development LLC</p> <p>Applicant Kevin Kramer, Manager Weekley Homes, LLC</p> <p>Project Planner TeNeika Neasman</p> <p>Updated: July 5, 2016</p>	<p>Property Location: 115 E Concord St. (south of Hillcrest St., east of N. Magnolia Ave., west of Highland Ave., and North of E. Concord St.) (Parcel ID # 25-22-29-3704-00-131) (±0.66 acres, District 4)</p> <p>Applicant's Request: The applicant is requesting PD rezoning from O-1/T and O-2/T zoning to allow for a 12 unit townhome development in the Lake Eola Heights neighborhood.</p>	<p>Staff's Recommendation: Approval of the request, subject to the conditions in this report.</p> <p>Public Comment Courtesy notices were mailed to property owners within 400 ft. of the subject property during the week of July 9, 2016. As of the published date of this report, no inquiries have been received from the public.</p>
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FUTURE LAND USE MAP

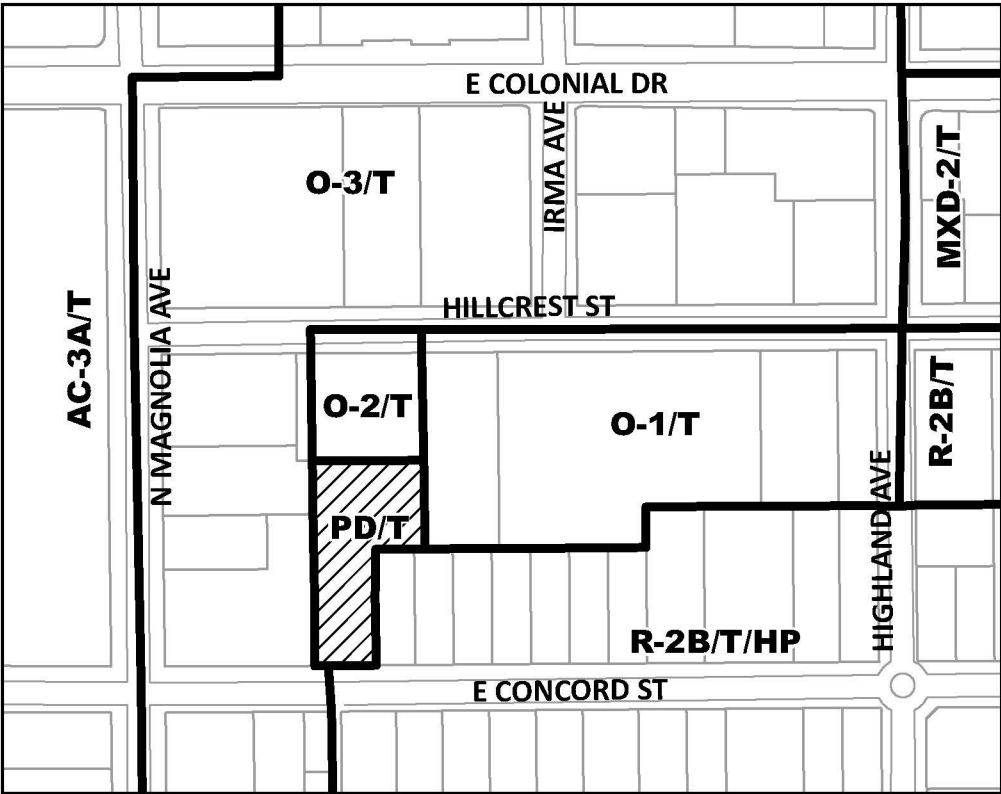


ZONING MAP





Zoning - Existing ZON2016-00013



Zoning - Proposed ZON2016-00013

PROJECT ANALYSIS

Project Description

The ±0.66 acre subject site is in the Lake Eola Heights neighborhood. The subject property is generally located south of Hillcrest St., north of E. Concord St., east of N. Magnolia Ave., and west of Highland Ave. The subject property is underdeveloped vacant residential land in the Lake Eola Heights neighborhood. Proposed is a 12 multi-story unit townhome development.

The site is zoned O-1/T and O-2/T (Office-Low and Medium/Traditional City overlay) and has a Future Land Use designation of Office Low and Office Medium. A rezoning to PD/T (Planned Development/Traditional City overlay) is proposed. The proposal is consistent with the existing Future Land Use and proposed Zoning designations.

Previous Actions:

- 1915– Property platted as part of the Chauncey Holts Revision Subdivision.
- 1988– Short Term (Minor) Plat of six parcels into one lot to develop a parking lot
- 2015– Property acquired by current owner

Project Context

As previously stated, the site is currently vacant. See Table 1– Project Context for details on surrounding uses. The site is surrounded by residential and office uses to the north, south, east, and west.

Table 1—Project Context

	Future Land Use	Zoning	Surrounding Use
North	OFFICE-MED	O-2/T	Office Coalition for the Homeless
South	RES-MED	R-2B/T/HP O-3/T	Office Single Family Home
East	RES-MED OFFICE-HIGH	R-2B/T/HP	Single Family Home Office
West	OFFICE-HIGH	O-3/T	Office

Conformance with the GMP

There are two Future Land Use Subarea Policies which border the site (S.14.3 and S.14.11) but neither pertain to the subject property. Per Future Land Use Figure LU-1, allowable uses in the Office Low Intensity and Office Medium Intensity are office, residential and public, recreational and institutional. The southern portion of the site has Office Low Intensity future land use (0.28 acre), which has no minimum intensity or density and a maximum density of 21 du/ac. and a maximum intensity of 0.4 FAR. The northern portion has Office Medium Intensity future land use (0.38 acre) with a maximum density of 40 du/ac. and a maximum intensity of 0.70 and a minimum density of 12 du/ac. and a minimum intensity of 0.30. the southern portion will have 6 units (11 du/ac.) which meets

Table 2—Development Standards

Acreage	Use	Sq. Ft./ Dwelling Units	Density (dwelling units per acre)		FAR (floor area ratio)		Building Height		ISR* (impervious surface ratio)	
			Max Allowed	Proposed	Max Allowed	Proposed	Max Allowed	Proposed	Max Allowed	Proposed
0.66	Residential	12 du proposed	21 du/ac	18.2 du/ac	N/A	N/A	30 ft. / 75 ft.	55 ft.	0.70/ 0.85	.74

PROJECT ANALYSIS

maximum density, while the northern portion will have 6 units which is within the required density range (15.8 du/ac.).

Conformance with the LDC/PD

The PD district is intended to provide a process for the evaluation of unique, individually planned developments which are not otherwise permitted in the zoning districts and provide superior design. The subject site is zoned O-1/T and O-2/T (Office Low and Medium/Traditional City overlay) on the Official Zoning Map, and would maintain a default zoning as such with the proposed PD rezoning. Table 2- Development Standards of this report details the maximum allowable density amongst other development standards. The proposed 12 units results in the maximum units for the southern portion (6 units) and a density within the range (6 units). A building height of 29 ft. is proposed with a parapet and architectural elements extending to approximately 30 ft. in height. The maximum height permitted in the O-1/T is 30 ft. and O-2/T is 75 ft.

City codes has specific lot standards and setback requirements for attached dwellings (townhomes). Tables 3 & 4 outline the required and proposed standards, which are being modified as a part of this PD request. The minimum lot width proposed is 20 ft. but all remaining lots range from 21-38 ft. in width. Front setbacks along E Concord St. and the mews internal to the site are 20 ft.

A Bufferyard 'A' (5-7 ft in width) is required along the southern and eastern property lines. The applicant proposes to provide the bufferyard width of 5 ft. with 80% tree coverage and vegetative cover and 6 ft. high PVC fence. The applicant is proposing a pre-cast wall along the eastern (Historic Preservation) property line, a 6 ft. high PVC fence along the northeast, north, and west property line. A maximum 4 ft. high decorative fence can be used on the lots (along the mews). Chain link fencing shall be prohibited. If a wall or opaque fencing is not provided as part of the required bufferyard, a hedge shall be provided. The hedge shall be a minimum of 4 ft. in height at the time of installation.

Table 3—Minimum Lot Standards		
	Required	Proposed
Minimum lot area	1600 sq. ft.	1,420 sq. ft.**
Average lot area	1800 sq. ft.	2,445 sq. ft.
Minimum lot width	20 ft.	20 ft.
**Does not meet code		

Table 4—Setback Requirements		
Yard	Required	Proposed
Front	20 ft.	20 ft.
Side	5 ft.	5 ft.
Rear	20 ft.	13 ft.**
**Does not meet code		

Transportation

The subject development will have access to the City's roadway network via one, two-directional driveway from on E Concord St.. The proposed 12 townhome units results in a trip generation of approximately 69.72 average daily trips. The development therefore did not merit a traffic study. Chapter 61 of the LDC requires 1 parking space for attached dwellings and 2 spaces when these dwelling exceed 2,000 sq. ft. of gross floor area. Nine of the proposed unit types (1-6 and 10-12) exceed a GFA of 2,000 sq. ft. but the applicant proposes a two-car garage for all of the units. The applicant is proposing 24 parking spaces with 5 additional parking spaces, one handicap space and 4 guest parking spaces.

There is currently on-street parking along E Concord St., south of the subject site, on both sides of the road. The on-street parking is restricted to two (2) hour parking from 8am to 6pm.

Urban Design

This project is located in the Downtown Community Redevelopment Area [CRA], an Major Certificate of Appearance Review from the Appearance Review Board is required prior to submittal of building permit. The streetscape shall be designed and constructed in accordance with the Downtown Streetscape Design Guidelines. The existing sidewalk shall be removed during construction and replaced with a 5-foot wide concrete sidewalk. The site plan shall be revised to show all ground mounted mechanical equipment screened with landscape that is a minimum 3-feet tall at the time of planting.

School Impacts

On July 7, 2008, the City adopted a Public School Facilities Element (PSFE) and the Amended Interlocal Agreement for Public

PROJECT ANALYSIS

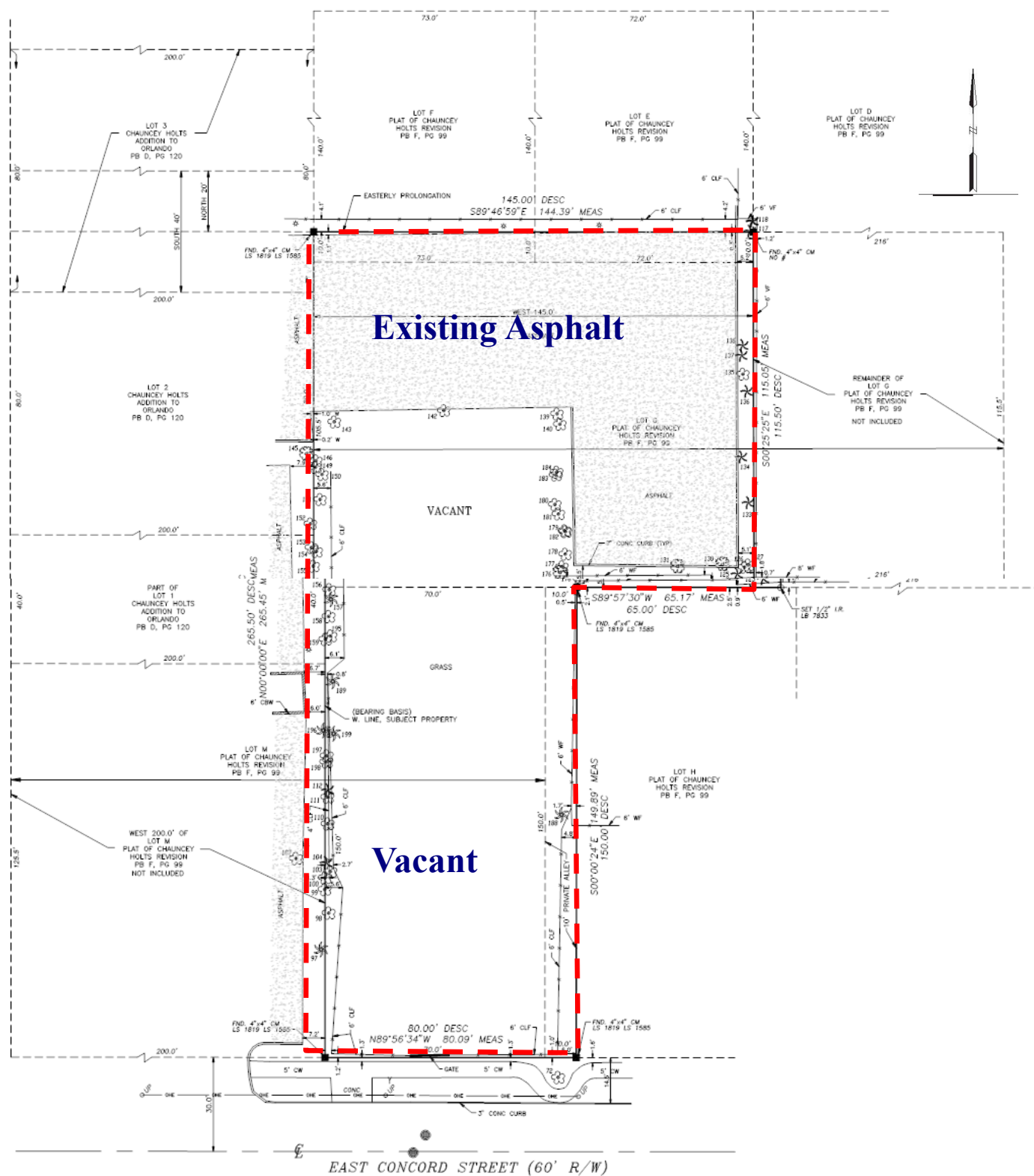
School Facility Planning and Implementation of Concurrency which requires all residential developments be subject to school concurrency review. Under the terms of the agreement, the City will advise OCPS of comprehensive plan amendments, zoning amendments, and development proposals that may have the effect of increasing existing density. A Capacity Enhancement Agreement is not required because this site was a part of a larger GMP amendment in 2010 which resulted in an overall reduction of units. Concurrency is required as there is no previous use on the site and thereby vested with 0 units.

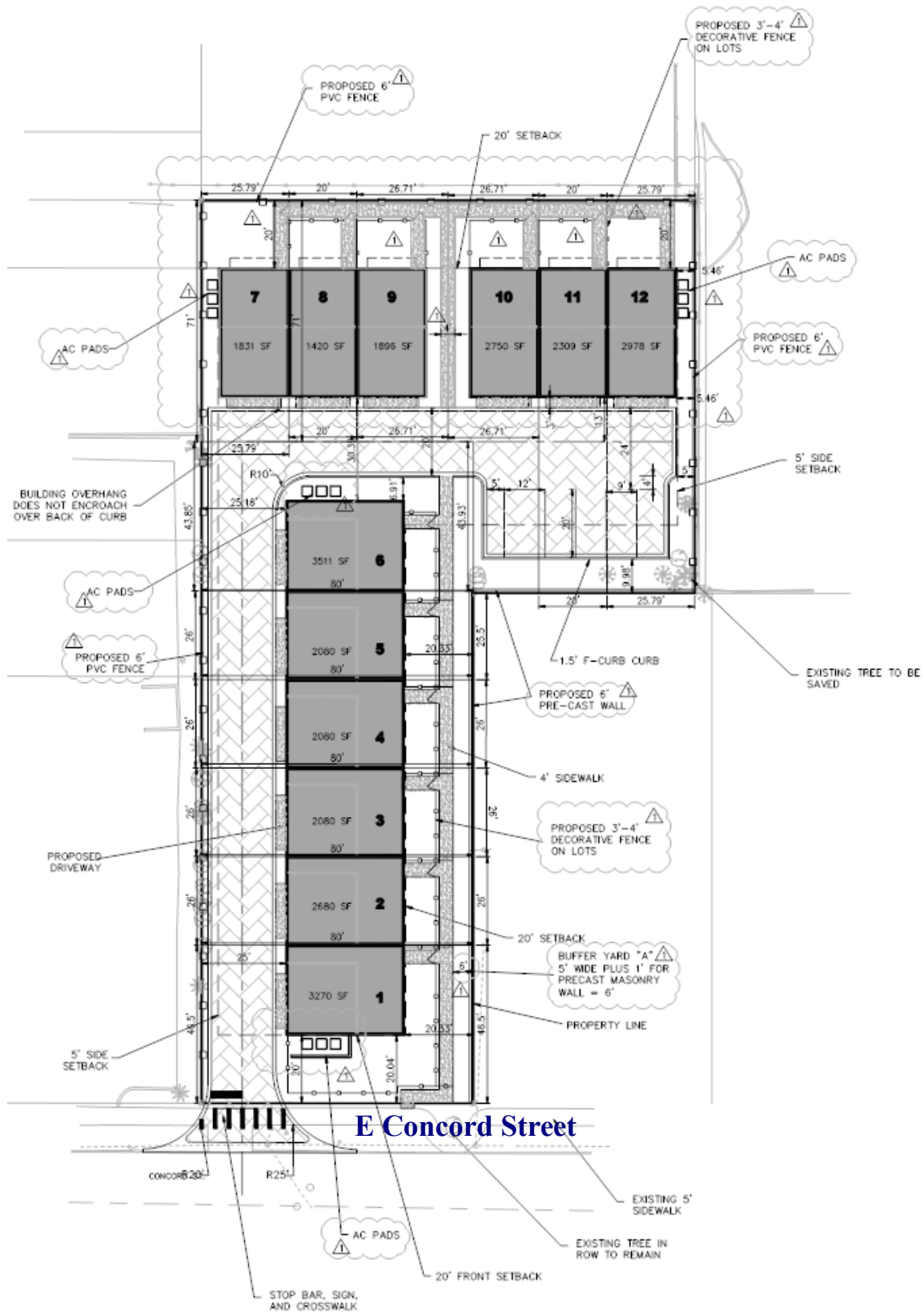
Additional Information

Solid Waste. The trash will be collected internal to the site.

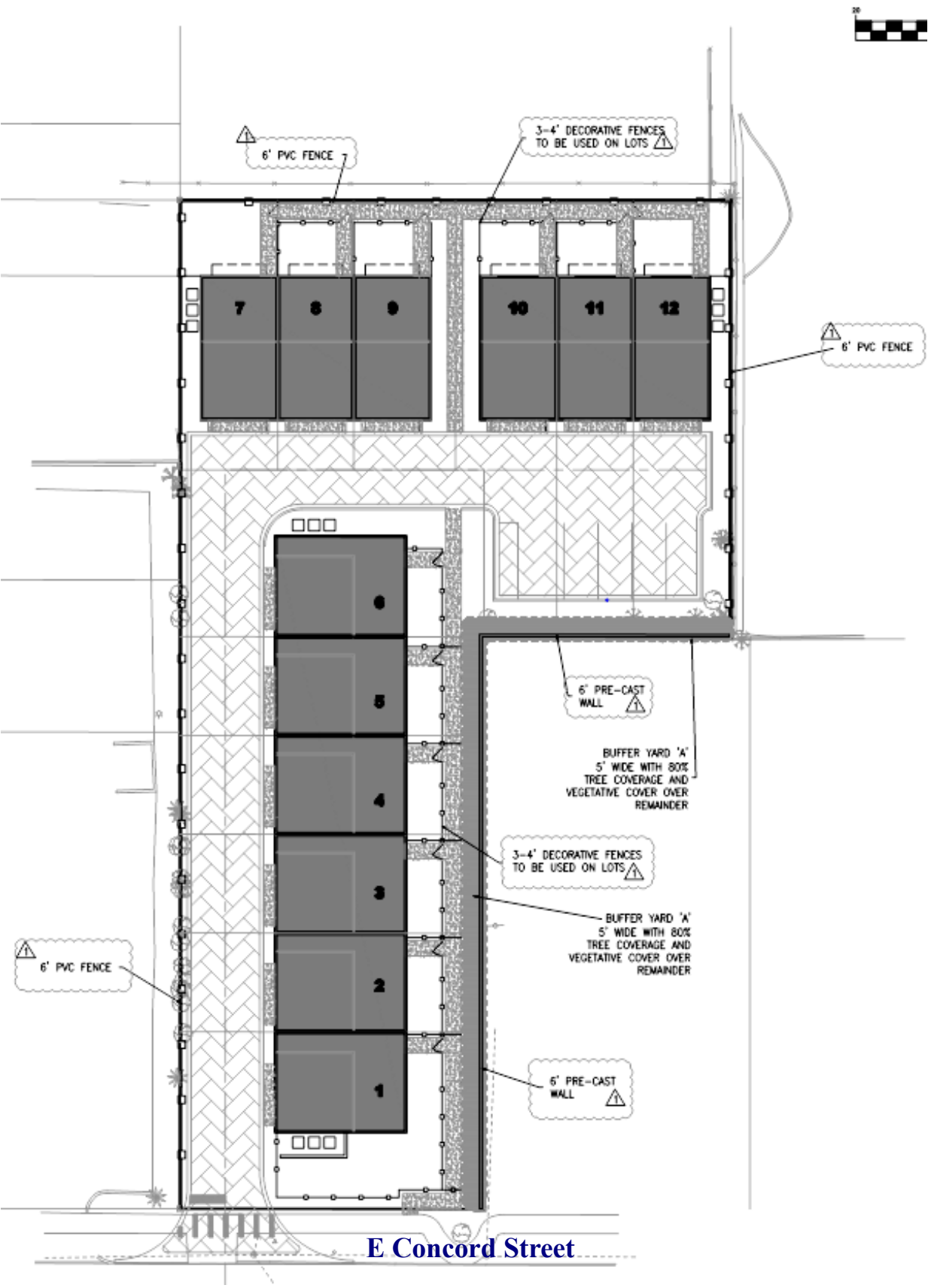
Storm Water. The storm water management system for this project will be an underground exfiltration system. Details will be provided with construction plan at time of permitting.

SURVEY

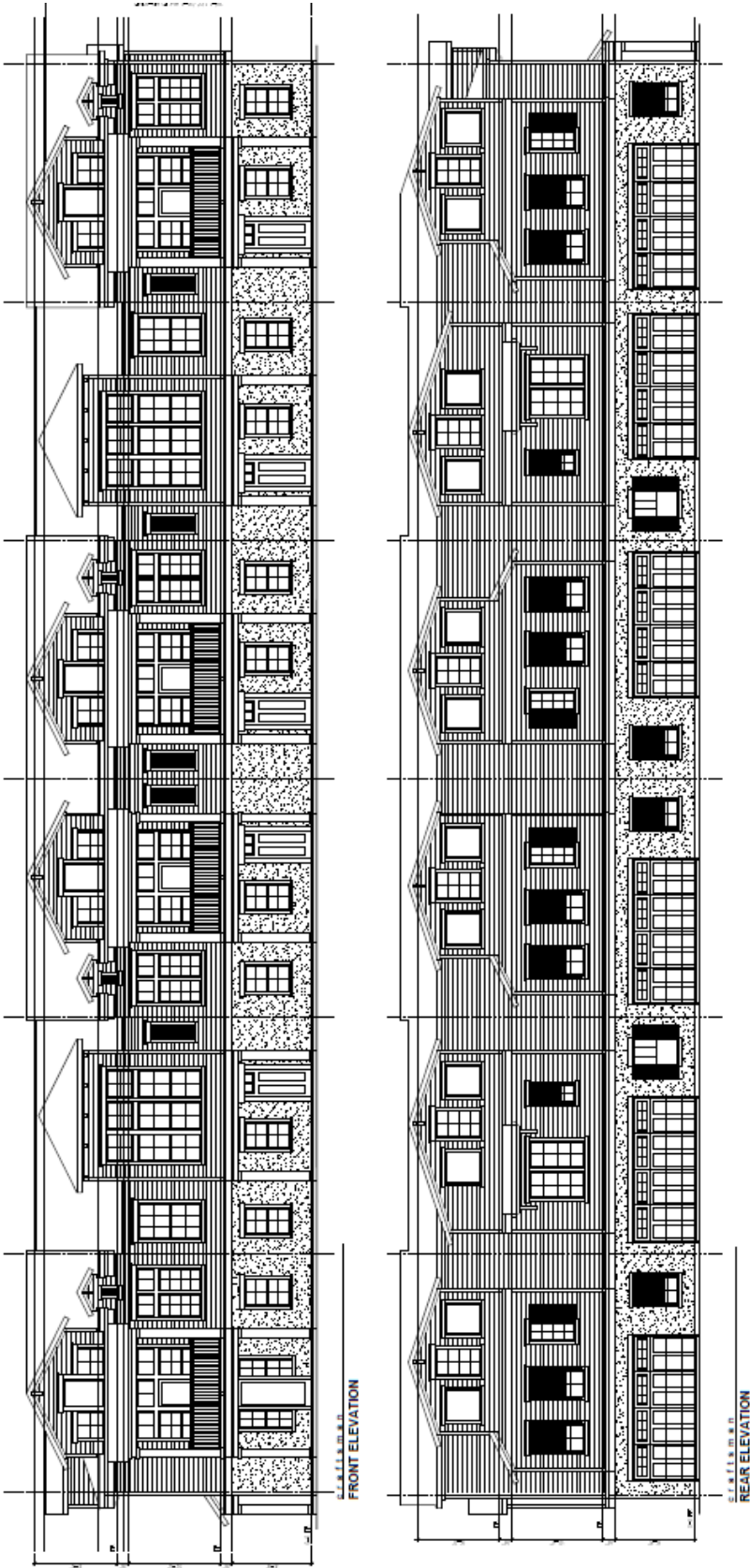




Landscaping Plan



Elevations



Elevations



craftsman
right ELEVATION



craftsman
left ELEVATION

Renderings



Mews on southeast portion of the site (along E Concord St.)



Driveway on southwest portion of the site (along E Concord St. entrance)

SITE PHOTOS

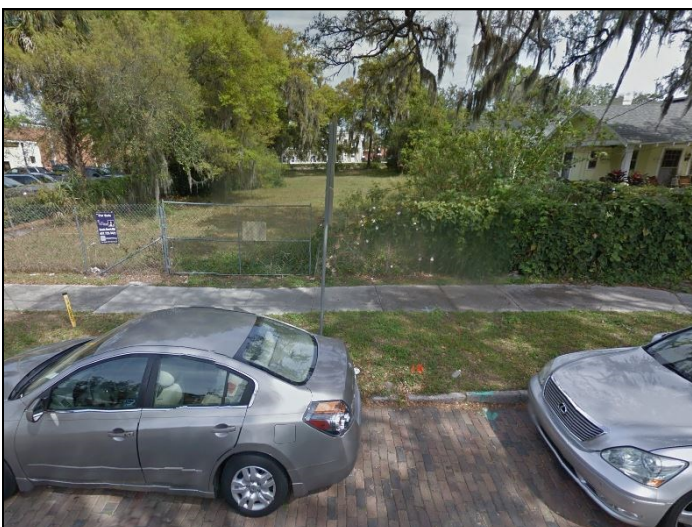
Property south of the subject site, across E. Concord St.



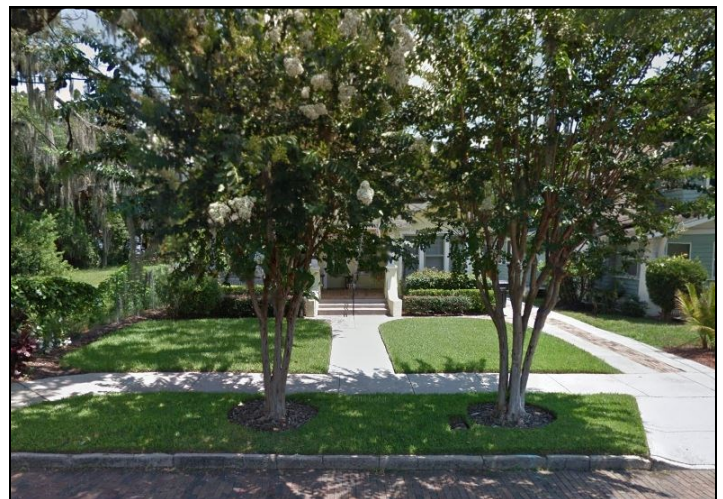
Property on the corner of E. Concord St. and Magnolia Ave.



Property west of the subject site.

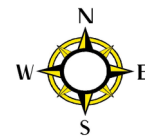


Subject site, along E. Concord St.



Property located east of the subject site.

AERIAL PHOTO



FINDINGS

Subject to the conditions contained herein, the proposal is consistent with the requirements for approval of Planned Development applications contained in Chapter 65 of the Land Development Code (LDC):

1. The proposal is consistent with the City's Growth Management Plan.
2. The proposal is consistent with the purpose and intent of the PD zoning district and all other requirements of the LDC.
3. The proposal is compatible with surrounding land uses and the general character of the area.
4. The necessary public facilities will be adequate to serve the proposed use, or will be provided by the applicant as a condition of this approval.

Staff recommends approval of ZON2016-00013 subject to the following conditions:

CONDITIONS OF APPROVAL

Land Development

1. Default Zoning. The underlying zoning shall default to the standards of the O-1/T and O-2/T zoning districts.
2. Conformance with PD Required. Construction and development shall conform to approved site plans, elevations, and landscaping plans on file with the City Planning Division and all conditions contained in this report, or as modified by the Municipal Planning Board and City Council. When submitting plans to the Permitting Division for permitting, the applicant shall attach to each submittal a copy of this staff report, and the excerpts of the Municipal Planning Board minutes.
3. General Code Compliance. Development of the proposed project should be consistent with the conditions in this report and all codes and ordinances of the City of Orlando, the State of Florida, and all other applicable regulatory agencies. All other applicable state or federal permits must be obtained before commencing development.
4. Minor Modifications. Minor modifications to the project, including changes to the design and site plan reviewed by the Municipal Planning Board or City Council may be approved by the Planning Official without further review by the Municipal Planning Board. Major changes shall require additional review by the Municipal Planning Board.
5. As provided by subsection 166.033(5), Florida Statutes, issuance of a development permit by a municipality does not in any way create any right on the part of an applicant to obtain a permit from a state or federal agency and does not create any liability on the part of the municipality for issuance of the permit if the applicant fails to obtain requisite approvals or fulfill the obligations imposed by a state or federal agency or undertakes actions that result in a violation of state or federal law. In accordance with subsection 166.033(5), Florida Statutes, it is hereby made a condition of this permit that all other applicable state or federal permits be obtained before commencement of the development.
6. Variances and modifications. Zoning variances and modification of standards may be approved pursuant to the procedures set forth in Part 2J and Part 2F, Chapter 65, Orlando City Code, respectively. The planning official may also approve minor modifications and design modifications to fences, walls, landscaping, accessory structures, signs, and bufferyard requirements. Additionally, recognizing that development plans can change in small ways between the planning and permitting stages of development, the planning official may approve up to a 10% modification of any applicable numerical development standard if the planning official finds that the proposed modification is consistent with the applicable goals, objectives, and policies of the GMP, is compatible with nearby existing land uses, would not result in inadequate public facilities, and is otherwise consistent with the public health, safety, and welfare. When approving such a modification of a development standard, the planning official may impose one or more of the conditions of development provided at section 65.334, Orlando City Code, but such condition or conditions must be reasonably calculated to mitigate the identifiable land use impacts of the modified standard.
7. Conformance with PD Required. Construction and development shall conform to approved site plans, elevations, and landscaping plans on file with the City Planning Division and all conditions contained in this report, or as modified by the Municipal Planning Board and City Council. When submitting plans to the Permitting Division for permitting, the applicant shall attach to each submittal a copy of this staff report, and the excerpts of the Municipal Planning Board minutes.
8. Review by City Attorney's Office. Municipal Planning Board recommendation of the conditions contained herein is subject to review by the City Attorney's Office for legal sufficiency and drafting of implementing documents.
9. Impervious surface ratio. The impervious surface ratio for the Property may not exceed the 0.74
10. Height. Maximum height shall not exceed 55 ft. not to include architectural detailing.
11. Lot Width. All lots are required to be a minimum of 20 ft. in width.
12. Lot Area. The minimum lot area permitted is 1,420 sq. ft. with a minimum average lot area of 2,445 sq. ft.
13. Setbacks. The minimum required setbacks for the overall property shall be as follows: 20 ft. on E Concord St., 20 ft. along the north property line (rear) and east (units 1-6) property line, 5 ft. along the east property line (unit 12), and 20 ft. along the east property line (units 1-6), consistent with the proposed site plan.
14. School Capacity. The project shall be subject to school concurrency and shall coordinate with OCPS.
15. Overhangs. Balconies and other such overhangs shall not encroach into the ROW of the utility/sidewalk easement.
16. Trash pick-up. Collection of trash bins shall occur internal to the site.
17. Bufferyard. If a wall or opaque fencing is not provided as part of the required bufferyard, a hedge shall be provided. Said hedge shall be a minimum of 4 ft. in height at the time of installation.
18. Accessory Buildings. Accessory buildings and pools shall be prohibited.

Urban Design

1. Streetscape
 - All Streetscape shall be designed and constructed in accordance with the Downtown Streetscape Design Guidelines.
 - All existing curbing shall be removed during construction and replaced with Type B vertical curb.
 - The existing sidewalk shall be removed during construction and replaced with a 5-foot wide concrete sidewalk.
2. Architecture
 - The entry stoop on the E. Concord facing end unit shall be converted to a porch. Per the LDC the porch shall be a minimum of

CONDITIONS OF APPROVAL

6-foot deep and open air on three sides.

The entry areas of the east facing townhomes shall also be expanded to be a minimum of 6-feet deep.

3. Mechanical Equipment and Venting

All ground mounted mechanical equipment shall be screened with landscape that is a minimum 3-feet tall at the time of planting.

All venting shall be directed to the rear or roof of the units.

Backflow preventers shall be located as far as possible from the right-of-way and shall be screened with landscaping.

Transportation Engineering

1. The final site plan shall show the location and size of the on-site solid waste compactor(s) / dumpster(s) with concrete pads, and enclosures with doors. The solid waste container(s) shall not be located adjacent to any single family houses or directly adjacent to the public street. Dumpsters shall be located to provide a minimum 50 feet of clear backup space and constructed per Orlando Engineering Standards Manual (ESM) requirements, OR documentation shall be provided from the City's Solid Waste Division indicating curb pick-up or other approved arrangement.
2. The minimum width for a two way drive aisle will be 23 feet.
3. Any interior sidewalks/pedestrian paths directly adjacent to a traveled way shall be a minimum 6 feet wide.
4. The driveway must be Orlando LDC and ESM requirements for distance from adjacent driveways.
5. Sidewalks on the west side of lot 7 and the east side of lot 12 will either be removed or continues as a logical pedestrian path.
6. Coordination with the City's addressing department will be required during permitting.
7. Provide an auto turn analysis of how a standard fire apparatus used by the City of Orlando will be able to access the entire site. Approval by City Fire Chief or designee will be required before a permit can be released.

Transportation Planning

1. A detailed pavement treatment or delineated pedestrian (sidewalk) connection shall be shown between building entrances, with a connection to be provided to the public sidewalk.
2. Provide written confirmation from the City's Fire Safety Engineer that the proposed access driveway and building figuration shall be able to accommodate emergency vehicle services.
3. Provide written confirmation from the City's Solid Waste Division Manager shall be provided that the proposed driveway and building configuration can be adequately served for regular scheduled garbage pick-up.
4. The Owner/Applicant shall provide documentation as to the steps that will be taken to protect the adjacent residential neighborhoods from construction cut-thru traffic and construction parking. Construction worker/equipment parking and materials staging shall be shown and noted on the permit plan submittal. MAINTENANCE OF TRAFFIC (M.O.T.) PLANS ARE REQUIRED.
5. For any construction work planned or required within a public right-of-way or City sidewalk easement adjacent to a public right-of-way (including but not limited to: irrigation, drainage, utility, cable, sidewalk, driveway, road construction/reconstruction or landscaping), the Owner/Applicant shall submit the following:
 1. Maintenance of traffic plans (M.O.T.) (For more information/detailed requirements contact the Office of Special Events & Permits at 407-246-3704)
 2. Roadway plans including paving, grading, pavement markings and signage (Contact the Permitting Transportation Engineering Reviewer at 407-246-3322 for details)

Waste Water

1. Each fee simple unit shall have its own lateral connection to the gravity main. Double wye connections or other means of combined laterals are not allowed. Proposed sanitary sewer shall be privately owned and maintained.

CONDITIONS OF APPROVAL-INFORMATIONAL

Transportation Impact Fee

1. For questions and information regarding Transportation Impact Fee rates you may contact Nancy Ottini at (407) 246-3529 or nancy.jurus-ottini@cityoforlando.net
2. Any new construction, change in use, addition, or redevelopment of a site or structure shall be subject to a review for Transportation Impact Fees. An estimated Transportation Impact Fee in the amount of \$26,280.00, based on the construction of 12 unit multi-family residential units, will be due at the time of building permit issuance, subject to change upon final permit plan review. For a copy of the complete ordinance or impact fee rate chart, you may reference our website at: <http://www.cityoforlando.net/planning/Transportation/ifees.htm>
3. The applicant shall comply with all applicable requirements of Chapter 59, the Concurrency Management Ordinance, to ensure that all public facilities and services are available concurrent with the proposed development, and that the potential impacts on public facilities and services are mitigated.
4. All new construction, changes in use, additions or redevelopment are required to submit a Concurrency Management application as a part of the building plan review process. A Concurrency Management application is available on the City's website: <http://www.cityoforlando.net/permits/forms/concurrency.htm>

OUC

1. Submit detailed water utility plans to Orlando Utilities Commission Development Services when they have been developed. See our Website for submittal information <http://www.ouc.com/business/water-services>.

Orlando Fire Department

1. The final design approach is viable. As this is not a full review, I would like to point out certain triggers to compliance with fire department access and fire protection systems.
2. Since the residential property contains 12 four (4) story buildings, each townhome unit will require a 13R automatic sprinkler system.
3. City Municode 24 requires four (4) hour fire wall separation between townhome units. A equivalency will be required to install a 2 hour fire wall with 13R automatic sprinkler system as an alternative to the Four (4) requirement.
4. The installation of the private hydrant and combination fire line will require UG Fire Permits and inspection by a fire safety inspector from permitting services.
5. The townhome community will be required to comply with FBC and FFPC codes

Orlando Police Department

1. The Orlando Police Department has reviewed the plans for Lake Eola Heights townhomes located at 115 E. Concord St., utilizing CPTED (Crime Prevention Through Environmental Design) principles. CPTED emphasizes the proper design and effective use of a created environment to reduce crime and enhance the quality of life. There are four overlapping strategies in CPTED that apply to any development: Natural Surveillance, Natural Access Control, Territorial Reinforcement and Target Hardening.

CONTACT INFORMATION

Growth Management

For questions regarding Growth Management plan review, please contact Mary-Stewart Droege at (407) 246-3276 or mary-stewart.droege@cityoforlando.net

Land Development

For questions regarding Land Development review, please contact TeNeika Neasman at (407) 246-4257 or teneika.neasman@cityoforlando.net.

Urban Design

For questions regarding Urban Design plan review, please contact Doug Metzger at 407.246.3414 or doug.metzger@cityoforlando.net

Transportation Planning

For questions regarding Transportation Planning plan review, please contact Nancy Ottini at 407-246-3529 or nancy.jurus-ottini@cityoforlando.net

Transportation Engineering

For questions regarding Transportation Engineering plan review, please contact Lauren Torres at 407-246-3220 or lauren.torres@cityoforlando.net.

Engineering/Zoning

For questions regarding Engineering or Zoning contact Keith Grayson at (407)246-3234 or keith.grayson@cityoforlando.net. To obtain plan review status, schedule/cancel an inspection and obtain inspection results, please call PROMPT, our Interactive Voice Response System at 407.246.4444.

Wastewater

For questions regarding Wastewater review, please contact Vince Genco at vince.genco@cityoforlando.net

Police

CPTED conditions will be emailed to the client by the Project Manager with the City. For questions regarding the Orlando Police Department plan review, please contact Audra Nordaby at 407.246.2454 or Audra.Nordaby@cityoforlando.net.

Building

For questions regarding Building Plan Review issues contact Don Fields at (407) 246-2654 or don.fields@cityoforlando.net.

Parks

For questions regarding Tree removal issues contact Justin Garber at (407) 246-4047 or justin.garber@cityoforlando.net

OUC

For questions regarding OUC issues contact Steven M. Lockington at 407-434-2568 or slockington@ouc.com

Transportation Impact Fee

For questions and information regarding Transportation Impact Fee rates you may contact Nancy Ottini at (407) 246-3529 or nancy.jurus-ottini@cityoforlando.net

REVIEW/APPROVAL PROCESS—NEXT STEPS

1. MPB minutes scheduled for review and approval by City Council.
2. City Council review and approval of First reading of ordinance.
3. City Council review and approval of Second reading of ordinance.
4. Applicant submits for building permits.