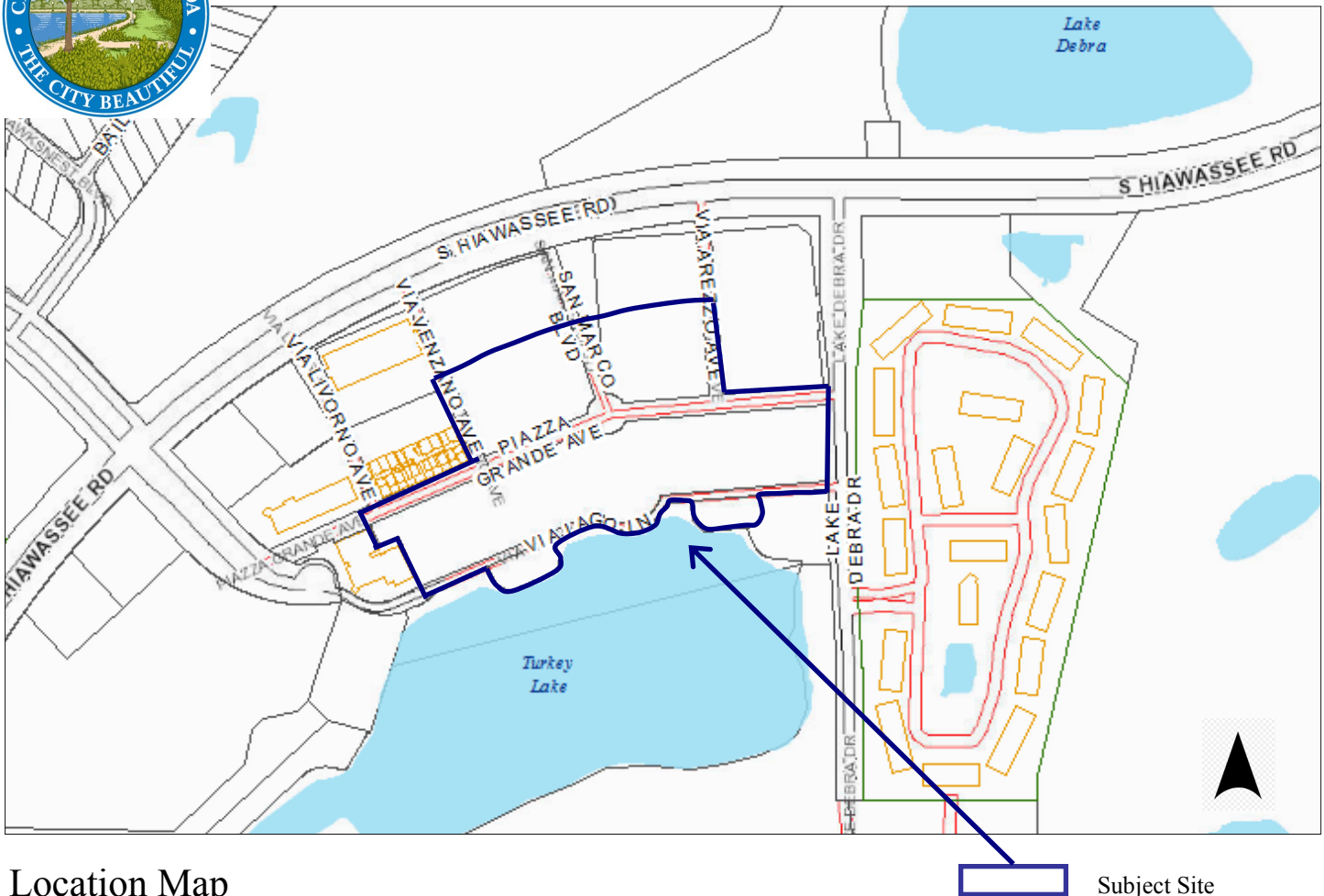




VERANDA PARK II RESIDENTIAL

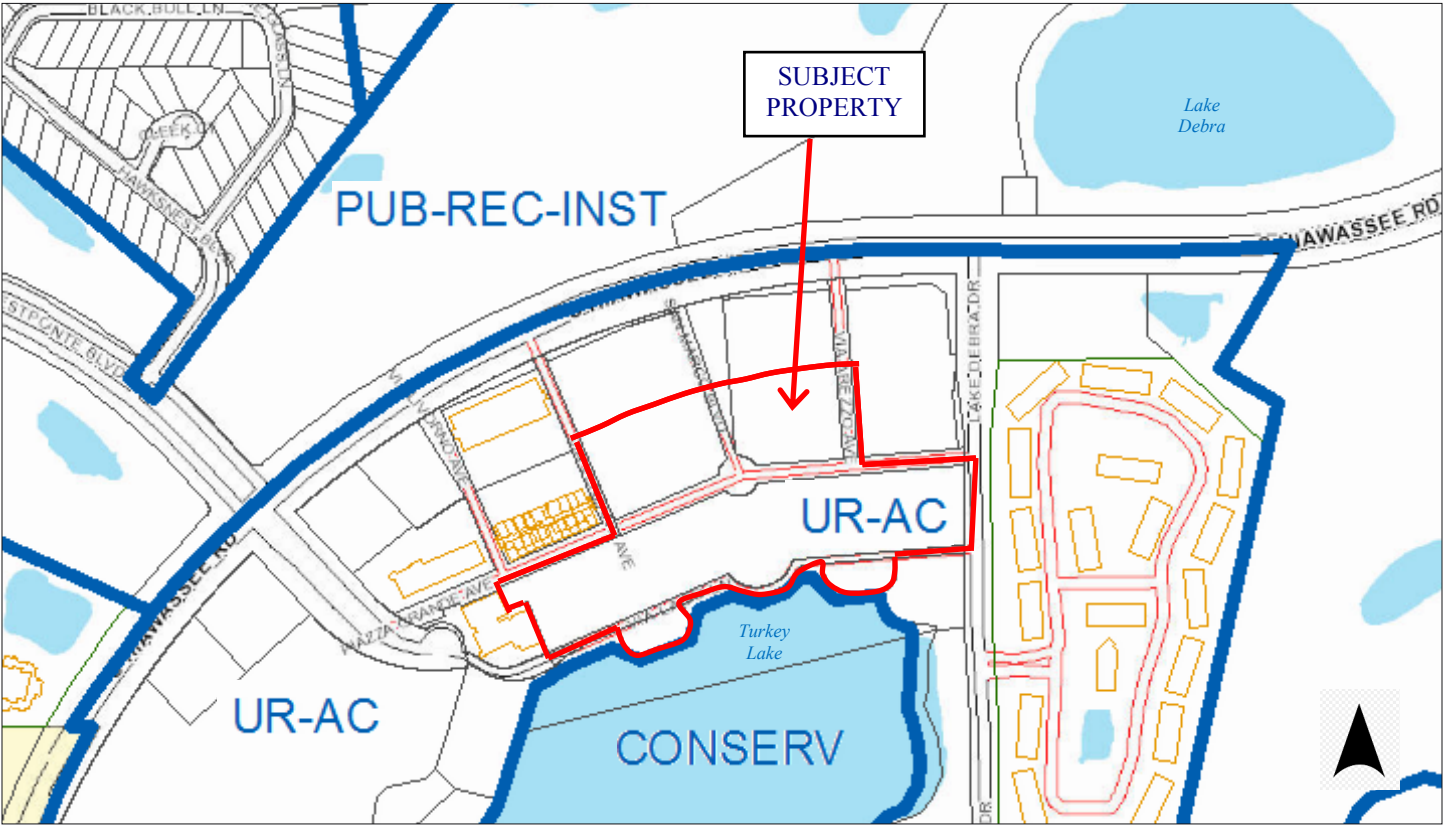


Location Map

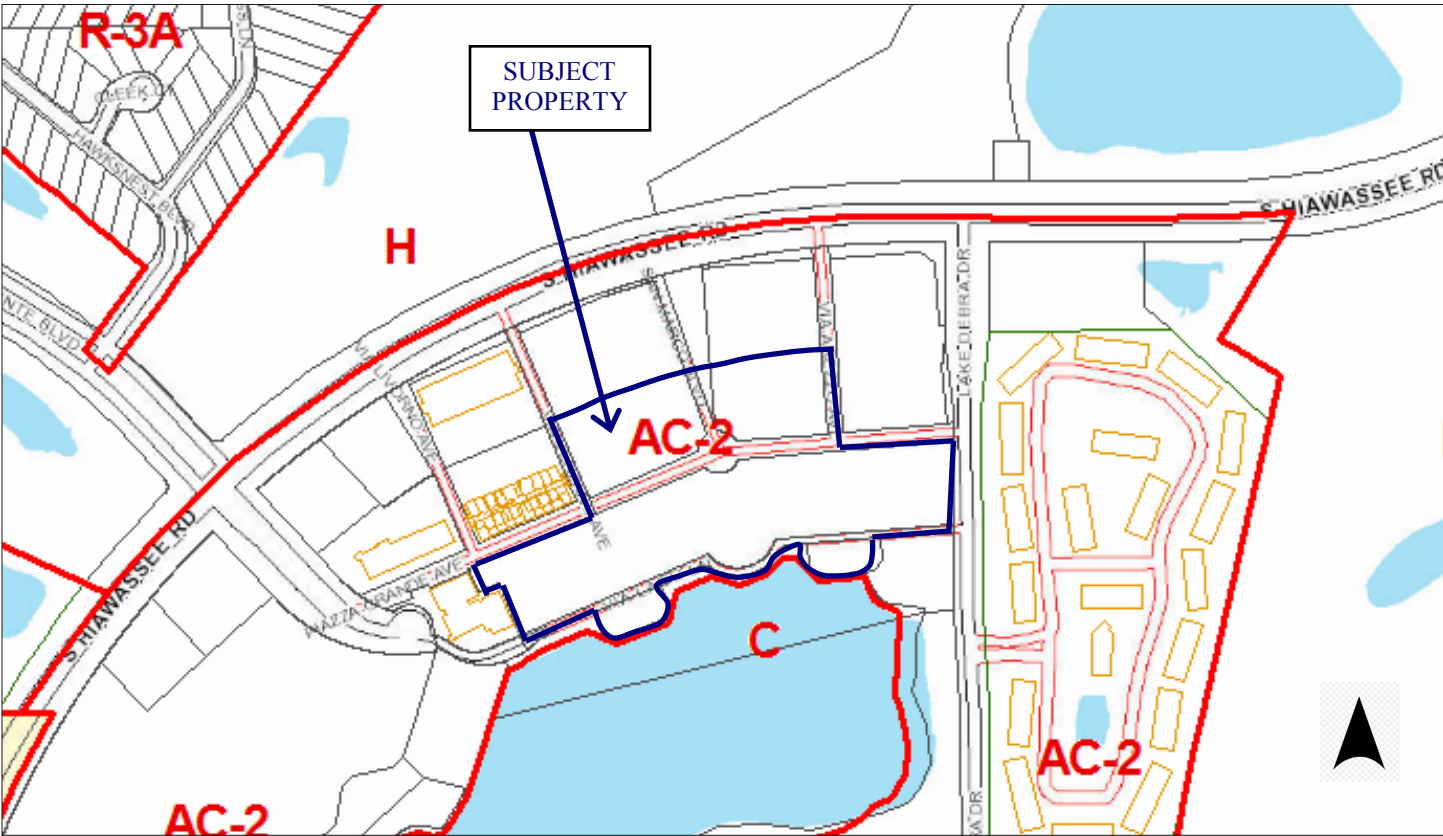
SUMMARY

<p>Owner</p> <p>James Stowers, Esq. Geosam Capital US, LP Owner's Representative</p> <p>Applicant</p> <p>Sam Sebaali, P.E., President Florida Engineering Group, Inc.</p> <p>Project Planner</p> <p>Katy Magruder, CNU-A</p> <p>Updated: May 10, 2016</p>	<p>Property Location: 2201, 2175 & 2141 S. Hiawasse Rd. 6951, 6920, & 6850 Via Lago Ln. (South of S. Hiawasse Rd. east and south of Via Livorno Ave., west of Lake Debra Dr.; Parcel IDs #02-23-28-8214-00-001, 02-23-28-8214-00-052, 02-23-28-8214-00-020, 02-23-28-8214-00-030, 02-23-28-8209-01-001, 02-23-28-8209-00-004, 02-23-28-8209-00-005, 02-23-28-8214-01-000) (±10.98 acres, District 6)</p> <p>Applicant's Request: Specific Parcel Master Plan request for the residential phase of the Veranda Park II development including four multi-family residential buildings totaling 642 dwelling units with parking and associated amenities. The development will occupy portions of Lots 2 and Tract A and all of Lots 3 & 5B, and Tracts E</p>	<p>& D as identified in the Veranda Park framework plan. Phase 2 will follow with an SPMP application for up to 45,000 sf of commercial space in the remaining portion of Lot 2 Tract A.</p> <p>Staff's Recommendation: Approval of the request, subject to the conditions in this report.</p> <p>Public Comment: Courtesy notices were mailed to property owners within 300 ft. of the subject property during the week of May 2, 2016. A public meeting was held at the Hair in Motion Studio on May 4, 2016. Parking was the primary of concern of the attendees. As of the published date of this report, staff received three inquiries from the public.</p>
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EXISTING FUTURE LAND USE MAP



EXISTING ZONING MAP



PROJECT ANALYSIS

Project Description

The ±10.98 acre subject site is located at the south of S. Hiawassee Rd., east and south of Via Livorno Ave., and west of Lake Debra Dr. The applicant is requesting Specific Parcel Master Plan approval for the development of four residential buildings, associated parking and amenities including pools, a pedestrian plaza, and a dog park. The site has Future Land Use designation of Urban Activity Center and a Zoning designation of AC-2.

Overview

This proposal, Veranda Park II Specific Parcel Master Plan (SPMP), will amend the Veranda Park II framework masterplan (MPL2015-00007) to include the existing 162,830 sq. ft. of commercial and office uses and 150 multifamily dwelling units as well as 45,000 sq. ft. of new commercial and 642 additional multifamily dwelling units, for a total of 207,830 sq. ft. of commercial and office as well as 784 multifamily dwelling units. An estimated ±10.16 acres of roadways, recreation tracts and drainage areas will also be included in this plan.

The site was the subject of a new mixed-use framework master plan June of 2015. As part of the June 2015 approval of MPL2015-00007 and DRI2015-00003, the applicant amended the Metrowest Development of Regional Impact (DRI) Development Order to remove the ±28.75 acre Veranda Park II Master Plan area owned by Geosam, LLC.

The Veranda Park II framework Master Plan (MPL2015-00007) also involved the reconfiguration of existing vacant lots (Lots 2, 3, and 5B). The new development program was comprised of the existing 162,830 sq. ft. of commercial and office uses and 150 multifamily dwelling units as well as 45,000 sq. ft. of new commercial and 710 additional multifamily dwelling units, for a total of 207,830 sq. ft. of commercial and office as well as 792 multifamily dwelling units.

Prior Actions

- 02/28/1983**—MetroWest DRI fully annexed into the City, concurrent with Project Debra Development Order (DO) approval (DOC.#17580-1).
- 02/11/1985**—City Council approved the first DRI DO amendment. The purpose of the amendment was to illustrate procedures governing the reallocation of the densities and intensities of uses between tracts (DOC.#17580-1).
- 03/12/1990**—City Council approved MetroWest DRI Amendment No 2. Changes, as part of this amendment, include a name change from Project Debra to MetroWest and a clarification of issues concerning the development of MetroWest as detailed in the DO; the Agriplex, described in the DO and the ADA, would not be developed (DOC.#17580-1).
- 02/25/2002**—City Council approved a subdivision plat to replat one lot into 5 lots for future development. The subject site is located on the south side of Hiawassee Rd. between Lake Debra Dr. and Westpointe Blvd. (±30.08 acres, SUB2002-00001; DOC: #020225A13).
- 06/18/2002**—Municipal Planning Board approved a mixed-use town center to include light retail, eating & drinking, office, civic and residential uses (±42.9 acres; MPL2002-00004).
- 08/20/2002**—Approval of abandonment of ROW for the terminus of Westpointe Blvd. to create a roundabout/cul-de-sac. The subject property to be abandoned was located at 7001 Westpointe Blvd. (±0.19 acres; ABN2002-00007).
- 05/30/2003**—Waiver from platting application to split one lot into two lots (SUB2003-00028), was subsequently determined that a waiver was not appropriate. Applicant then filed for a minor subdivision under SUB2003-00031.
- 06/02/2003**—City Council approved a Minor Plat - Veranda Park - 7001 Westpointe Blvd. : Included shifting lot lines of lots 5 and 6. (SUB2003-00031; DOC. #030602A15).
- 05/03/2004**—City Council approved a Minor Subdivision Plat -Veranda Park Second Replat - 2101 S. Hiawassee Rd. Replat of Tracts B and F into Tracts B, H & F (SUB2004-00016; DOC. #040503B06);
- 05/18/2004**—MPB approved a Growth Management Plan Amendment to change the future land use designation from Urban Activity Center to Conservation to accommodate water retention (GMP2004-00022). Subject property was also rezoned to "C" to reflect changes (ZON2004-00018).
- 09/21/2004**—MPB approved master sign package for Veranda Park (MPL2004-00018).
- 02/27/2006**—City Council approved a Minor Subdivision Plat -Veranda Park Third Replat. (SUB2005-00083; DOC. #060227C09).
- 06/16/2015**—MPB approved Master Plan amendment request to remove Veranda Park from the MetroWest DRI and to create out-parcels along S. Hiawassee Rd. and above ground residential uses via a framework Master Plan. (MPL2015-00007, DRI2015-00003)
- 09/21/2015**—City Council approved the Third Amendment to the Metrowest DRI Development Order (DRI2015-00003)
- 10/05/2015**—City Council Approval of the Hearing Officer's Recommended Order on Motions to dismiss QJ 2015-001 AND accepting the meeting minutes of the June 16, 2015 MPB meeting with respect to MPL2015-00007

PROJECT ANALYSIS

Project Context

The partially developed subject property is located north of Turkey Lake, south and east of Hiawasse Rd., and west Lake Debra Dr. Adjacent uses, zoning and future land use designations are shown in Table 1 below.

Table 1—Project Context			
	Future Land Use	Zoning	Surrounding Use
North	Public/Recreational/Institutional (PRI)	P (Public Use)	MetroWest Golf Course
East	Urban Activity Center (URBAN-AC)	AC-2 (Urban Activity Center)	Multifamily residential
South	Conservation (CONSERV)	Conservation (C)	Retention/Turkey Lake
West	Urban Activity Center (URBAN-AC)	AC-2 (Urban Activity Center)	Commercial: CVS, Chevron, Gas Station, and MetroWest Village Shopping Center

Conformance with the GMP

Surrounding future land use designations include Urban Activity Center and Public, Recreational and Institutional to the north, Conservation to the south and Urban Activity Center to the east and west. See chart above for details.

Background

The original ±42.93 ac. 2002 master plan program was to be comprised of 383,296 sq. ft. of commercial, office and civic uses and 550 residential units. As of 2015, an estimated 358,466 sq. ft. of commercial and office uses as well as 150 MF dwelling units have been built in the entire Veranda Park area.

In June 2015, MPB approved a new ±28.75 ac. Veranda Park II framework master plan (MPL2015-00007 and DRI2015-00003), which included the following changes:

1. Lot Reconfiguration: Three new lots 2, 3, and 5B were created. Developed Lots 4, 5A, 6 and 7, while included, were not changed. An estimated ±10.1 acres of roadways, recreation tracts and drainage areas was incorporated into the new framework plan and, as shown in this SPMP, involves a new internal roadway and pedestrian network.
2. Modified Development Program: The 2015 Veranda Park II development program is comprised of two parts: a) 162,830 sq. ft. of existing commercial/office uses and 142 multifamily dwelling units; and, b) 45,000 sq. ft. of new commercial and 710 additional multifamily dwelling units. Under this development scenario, at build out, there would be a total of 207,830 sq. ft. of commercial/office and 852 multifamily dwelling units in Veranda Park II.
3. Removal from the MetroWest DRI and the 2002 Veranda Park Master Plan: Also as part of the 2015 the Veranda Park II master plan approval, the new Master Plan area was removed from the MetroWest DRI and Lots 1A and 1B (2295 S. Hiawasse Rd. and 6965 Piazza Grande Ave.) and a ±19.5 acre undeveloped tract (PID: 36-22-28-5601-00-104), all under separate ownership, would remain in 2002 Veranda Master Plan and within the MetroWest DRI.
4. Common Areas: Access, parking and stormwater are shared between the two master plan areas and, as described by the applicant, is administered by the Veranda Park (property owners) Association.

Proposed Development

The proposed 642 dwelling units will be sited in four multistory buildings with parking garages integrated into each building on Master Plan Framework Lots 3 and 5B and a portion of Lot 2 and Tract A. Recreation areas are proposed in Tract D and E. See image on the following page for reference. As detailed in the site plan and application letter, the central plaza which runs north-south, will provide a view shed to Turkey Lake and serve as an organizing element for the various residential buildings, pedestrian and vehicular network, as well as the two recreational tracts to be developed by Turkey Lake. The proposed design is consistent with the Veranda Park II framework plan and the early new urbanist 2002 Veranda Park Master Plan.

The proposed SPMP site is located within the ±28.75 ac. Veranda Park II area. The applicant is proposing to add 642 dwelling units. Addressing the density of the Veranda Park II development site (lot) area (±18.58), the total development density at build out is 784 du or 58.5 du/ac., which meets AC-2 standards.

PROJECT ANALYSIS

School Concurrency

As it concerns school concurrency, the 2002 Veranda Park master plan development program was vested for 550 MF dwelling units. A total of 142 units have been built. As part of the framework Master Plan, a total of 852 dwelling units was anticipated at build out. In 2015, the applicant submitted an application to address an additional 302 dwelling units (ORL-15-005 Veranda Park). A school concurrency determination (denial) letter was issued by Orange County Public Schools on June 5, 2015. The denial was based on insufficient school capacity at the affected Concurrency Service Areas (CSA) to support the development of 302 multi-family residential units and would accordingly require an executed Concurrency Mitigation Agreement.

Accordingly, an executed agreement for 242 unvested MF dwelling units (for a total of 792 dwelling units at build-out) will be required at the time of platting or final site plan.

Conformance with the LDC

The development site has existing zoning designation of AC-2. “The AC-2 district is intended to provide for concentrated areas of residential, commercial, office, industrial, recreational and cultural facilities serving major subregions of the Orlando urban area, and at intensities significantly higher than in surrounding neighborhoods. Although some Urban Activity Centers may be composed of a single type of use, a mixture of land uses is specifically encouraged. These activity centers are intended for locations where arterials and four lane collectors and mass transit service are available, providing access between metropolitan subregions and complementing the primary arterial transportation system.”

A total of four multifamily residential buildings including 642 dwelling units and 1,429 parking spaces are proposed for the development of Lots 2, 3, 5B, and Tract A, D & E of Veranda Park. The development will result in a Floor Area Ratio (FAR) of 0.52 which is below the maximum permitted 1.0 FAR and above the minimum 0.5 FAR within the AC-2 zoning district. The overall development is also below the maximum Impervious Surface Ratio (ISR) of 0.90 for the AC-2 zoning district, measuring 73%.

There are two building types proposed for this development, Building Type I = Building 5000 and Building 5500 & Building Type II = Building 2000 and Building 3000. Building Type I is proposed at 52.8 ft. with architectural features reaching 72.8 ft. in height. Building Type II is proposed at 42.2 ft. to the top of the 5th floor. The applicant proposes a parapet to be constructed at the top of the building to shield the top floor parking deck and associated outdoor lighting. This parapet is proposed to extend to 60.2 ft. in height. Lastly, architectural features are proposed that will reach 70.2 ft. in height, resulting in compliance with the 100 ft. maximum height for the AC-2 zoning district. See Table 2– Development Standards for more information.

Table 2—Development Standards (AC-2 Zoning District)

<i>Sq. ft.</i>	<i>FAR (floor area ratio)</i>		<i>Building Height</i>		<i>ISR (impervious surface ratio)</i>	
	<i>Minimum / Maximum</i>	<i>Proposed</i>	<i>Maximum</i>	<i>Proposed</i>	<i>Maximum</i>	<i>Proposed</i>
Building 5000 & 5500(137,528 sf) Building 2000 & 3000 (114,092 sf) Total 251,620 sf for FAR + Brick Pavement (39,880 sf) Sidewalks + Pavers (51,755 sf) Existing Recreation Area Tract D (10,746 sf) Existing Recreation Area Tract E (8,940 sf) Existing asphalt to be removed (-12,971 sf) Total 349,970 sf for ISR	0.5 to 1.0	0.52	100 ft.	Building Type I = 72.87 ft. Building Type II = 70.16 ft.	90%	73.17%

Development Standards.

Minimum setbacks in the AC-2 zoning district are 10 ft. in the rear and 0 ft. or 3 ft. in the side yards and 0 ft. or 3 ft. for the front yard setback. The proposed development is compliant with the minimums and no buffers are required between the adjacent uses.

PROJECT ANALYSIS

Transportation

Access to the City's roadway network from Veranda Park II is by private streets that open directly onto S. Hiawasse Rd. and Lake Debra Dr. The original Veranda Park street network is partially built. The network originally contemplated a series of north/south street connections from Hiawasse Rd. to an east/west street that extends through the interior of the property from Lake Debra Dr. to Westpointe Blvd. Due to the reconfiguration of the site and the creation of new Lots 2 and 3 and reconfiguration of Lot 5B, the new development will involve the changing of San Marco Blvd. into an access driveway or shorter private road.

A new cross access will also run east west between Lots 2 and 3 (This road will be constructed in the next phase of development along with the 45,000 sq. ft. of commercial on Lot 2—unless fire deems it necessary to provide an access road in the meantime).

Vehicular access into the building garages for Buildings 2000 and 3000 will be from Via Venzano Ave. and Via Arezzo Ave. The applicant has assured staff that one entrance to the 426 parking spaces for each Building 2000 and 3000 is sufficient.

A 24 ft. driveway within a cross-access easement is proposed for future vehicular and pedestrian connections and will be an east west connector between Via Venzano Ave. and Via Arezzo Ave. This driveway will be constructed with the future commercial development, since there are currently adequate pedestrian and vehicular access points to the proposed project. The future driveway will service both the commercial and residential developments. This cross-access easement will include two 12 ft. paved vehicular travel lanes. Head curbs along the paved travel lanes and 6 ft. sidewalks are proposed on each side of the travel lanes within the limits of re-configured Lots 2 and 3.

The east-west Via Lago Ln. will be retained as a pedestrian promenade, and will be accessible to emergency vehicles. The entrances to the parking garages for Building 5000 and Building 5500 will be off of Via Lago Ln. The applicant has moved the bollards indicating the pedestrian promenade further towards the center to ensure that three cars can queue on Via Lago Ln. to prevent congestion on Lake Debra Dr. and associated side streets.

Piazza Grande Ave. is currently partially paved and will remain as the development's east-west spine road. At the time of development, all private streets must meet public street standards, including code compliant paved surfaces and sidewalks.

While centrally located, the LYNX bus transit system currently does not operate routes near the subject property and no significant transportation improvements are planned for this area. Per MPL2015-0007, "to assess network impacts at the time of SPMP submittal, each submittal, addressing one or more lots will be required to submit a traffic operations study addressing the two existing traffic signals at Westpointe Blvd. and S. Hiawasse Rd. and at Lake Debra Dr. and S. Hiawasse Rd." These traffic studies may be conducted post MPB and City Council approval, but prior to submitting for building permits.

Parking History

The 2002 master plan parking program included 3,537 parking spaces: 3,405 spaces were to be in parking structures and approximately 132 spaces along the street or in surface parking. Parking structures were to be located within five of the nine buildings and range in size from 160 to 850 spaces. A 2003 Veranda Park Development Agreement (Doc.#030519A17) detailed that parking within the development shall be calculated on a gross basis, taking into consideration the entire development. The minimum parking rates were: 2.5 parking spaces for each one thousand gross sq. ft. of commercial building area (including office) and 1.7 parking spaces per each residential dwelling units. Existing development follows these standards, as shown in the chart on the following page.

Per MPL2015-00007, the next developer of Veranda Park is responsible for replacing the parking spaces serving existing development and which are subsequently removed for development. The owner and applicant of this SPMP request have agreed to this condition. Table 4 below includes the most up to date parking calculations for the Veranda Park development. There are two temporary parking areas Lot 2 and Lot 5B which will be removed at time of development. The parking required for all existing development within Veranda Park is 1,137 parking spaces. Previously there was an 8-unit addition to the multifamily building on Lot 4. The applicant is responsible to include the required 14 parking spaces for the 8 units within Phase 1 of this development.

The requested SPMP requires a total of 1,124 parking spaces for the end of Phase 1 at the 1.75 spaces per dwelling unit rate identified in MPL2015-0007. In addition to the 1,137 parking spaces for the existing development and the 14 missing spaces, at buildout, there shall be 2,275 parking spaces. The applicant proposes 2,296 to provide an additional 21 parking spaces than required by the Master Plan.

For Phase 2, the owner and applicant will provide ample parking for the proposed 45,000 sq. ft. of retail at the 2.95 parking spaces per 1,000 sq. ft. of commercial space.

PROJECT ANALYSIS

Bicycle Parking

Short-term bicycle storage will be located at the entrance to each amenity/pool and at the plaza area in front of Building 3000 north of Piazza Grande Ave. Long-term bicycle storage will be located on the ground floor of the parking levels in each building. Per City Code Chapter 61, Part 3D, all developments are required to provide Bicycle Parking spaces for the use of patrons and employees. This project shall provide at least 11 long-term and 11 short-term bike parking spaces each in Buildings 5000 and 5500. 5 long-term spaces and 5 short-term spaces shall be provided in each of the other 2 buildings.

Table 4—Parking Requirements (Veranda Park II)

Development	Program (ksf or DUs)	Required Spaces	Existing Permanent Parking Spaces	(Deficit) or Surplus
Existing Development ¹				
Building 1000 (1A; Lot 1A)	96.7	242	55	(187)
Building 1500 (1B; Lot B)	99.0	247	67	(180)
Building 7; Lot 6	65.1	105	37	(68)
Building 8; Lot 5A	42.2	139	0	(139)
Building 4000; Lot 4	142 DUs	241	506	102
Building 4000; Lot 4	65.1	163		
Lot 7		0	126	126
On Street parking		0	76	76
Existing SubTotal ²		1,137	867	(270)
Proposed Development ³			Proposed Parking Spaces	
Buildings 2000 & 3000; Lot 3	202 DUs	354	852	498
Buildings 5000 & 5500; Lot 5B	440 DUs	770	554	(216)
Building 4000; Lot 4	8 DUs	14	0	(14)
On Street Parking ⁴		0	23	23
Phase 1 SubTotal		1,138	1,429	291
Development Total after Phase 1		2,275	2,296	21
Proposed in Next Phase				
Lot 2	45.0	133	144	11
Phase 2 SubTotal		133	144	11
Development Total after build-out		2,408	2,440	32

¹ Space Requirements per original PD Ordinance: 2.5 spaces per ksf for Commercial & 1.7 spaces per DU.

² Deficit currently met by two temporary, surface parking lots on parcels 2 and 5B. These will be removed for new construction.

³ Parking Space requirements per Master Plan conditions: 2.95 spaces per ksf for commercial & 1.75 spaces per DU.

⁴ The 8 DUs on Lot 4 have already been built in Building 4000, but the 14 associated parking spaces are being accounted for in this

Parking Plan

The owner/applicant addressed the parking by providing a site plan showing the additional parking spaces to be utilized by the existing retail and office components in Veranda Park. As shown in the site plan on the following page, Buildings 5500, 2000, and 3000 will have parking spaces to accommodate the existing commercial uses. This is also detailed in Table 4—Parking Requirements.



On-Site Amenities

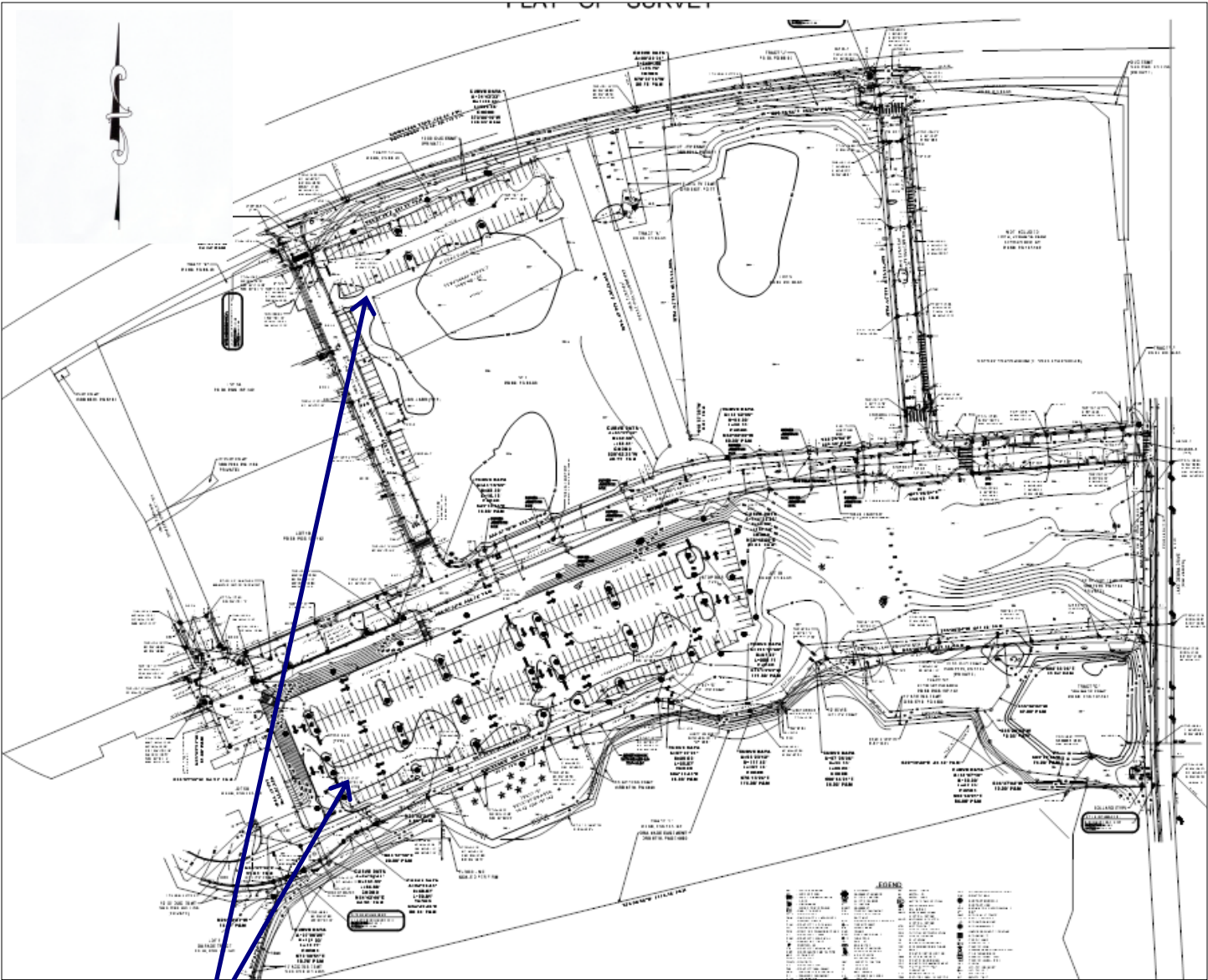
Signage

Stormwater

Water and Sewer Lines

This project is proposing to connect to the existing water and sanitary sewer utility infrastructure on Piazza Grande Avenue, which was previously constructed as part of the Veranda Park master plan development to serve the affected lot areas. The conceptual water and sanitary sewer utility design is shown on Sheets C-6A and C-6B of the Specific Parcel Master Plan submittal.

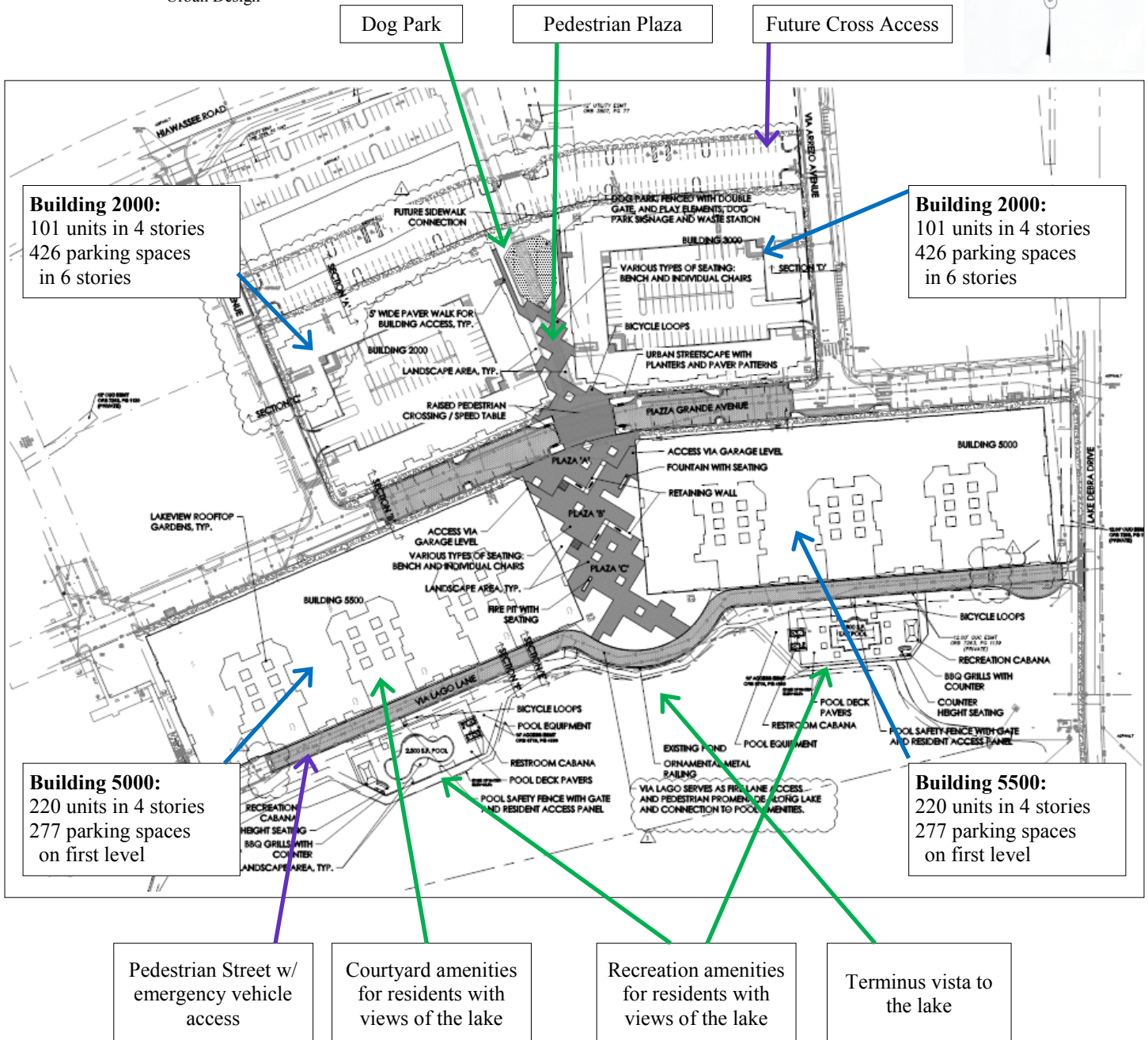
APPLICANT'S SURVEY



Temporary surface parking to be replaced.

SITE PLAN

- Planning & Zoning
- Transportation
- Urban Design



ELEVATIONS—BUILDING 2000 & 3000

(MIRROR IMAGE)



BLDG. II - FRONT ELEVATION

ELEVATIONS—BUILDING 5000 & 5500

(MIRROR IMAGE)

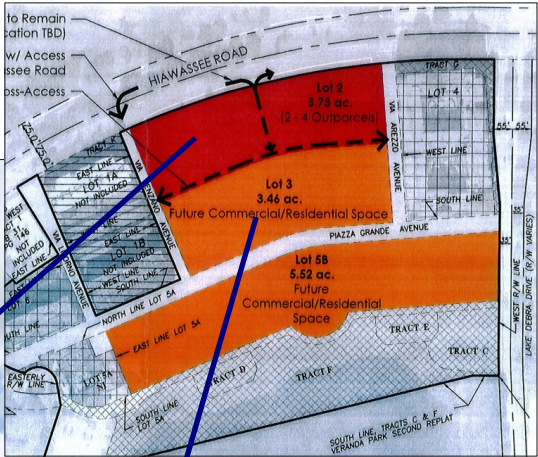


BLDG. I - FRONT ELEVATION



SITE PHOTOS

NW of Lot 2 Existing Temporary Surface Parking

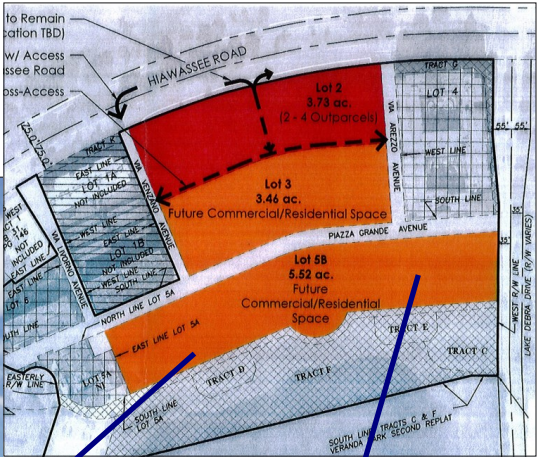


Facing East Towards Vacant Lot 3. Building Exists on Lot 4



SITE PHOTOS

Lot 5B—Proposed location of Building 5500



Lot 5B—Proposed location of Building 5000



SITE PHOTOS

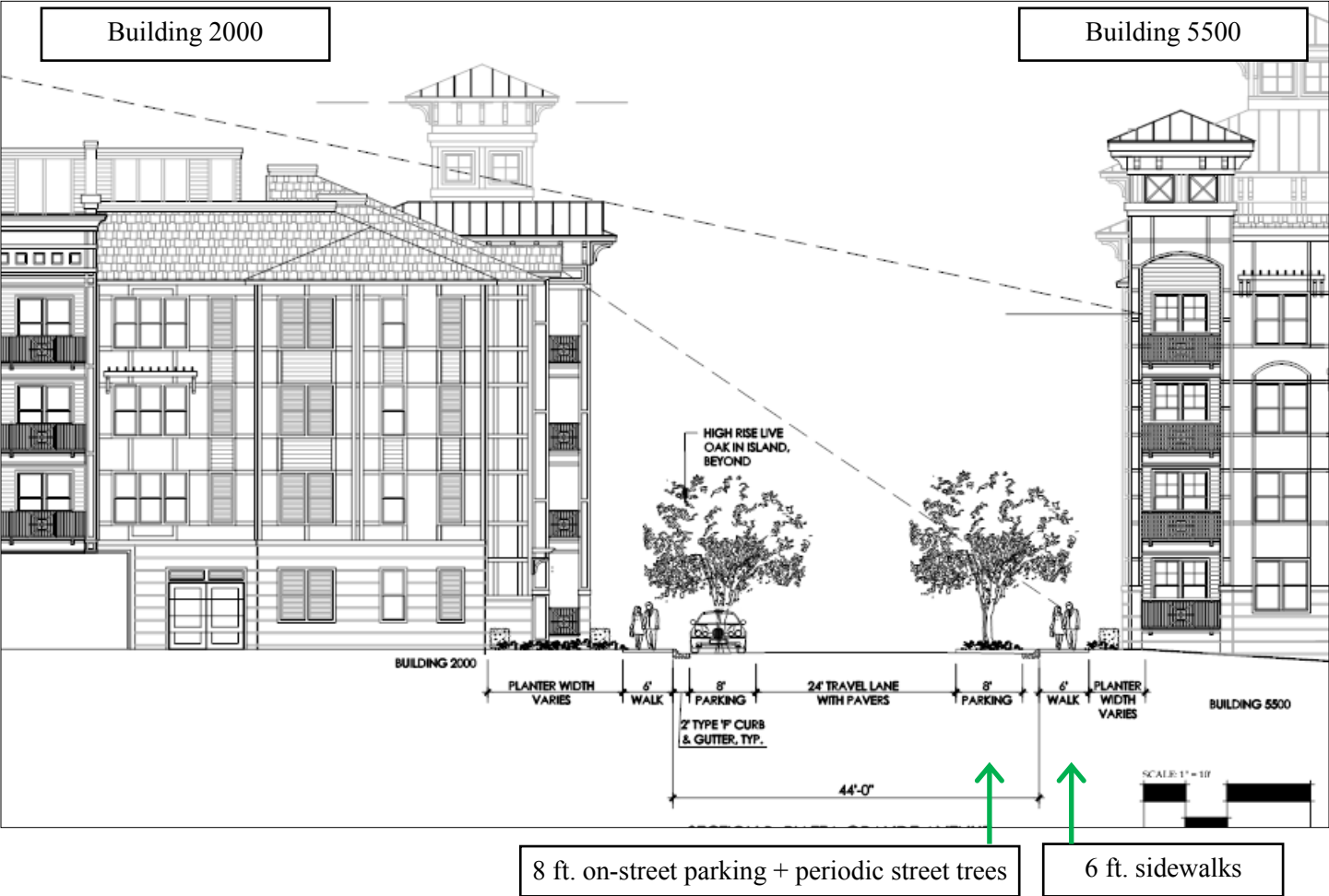
Via Venzano Ave.



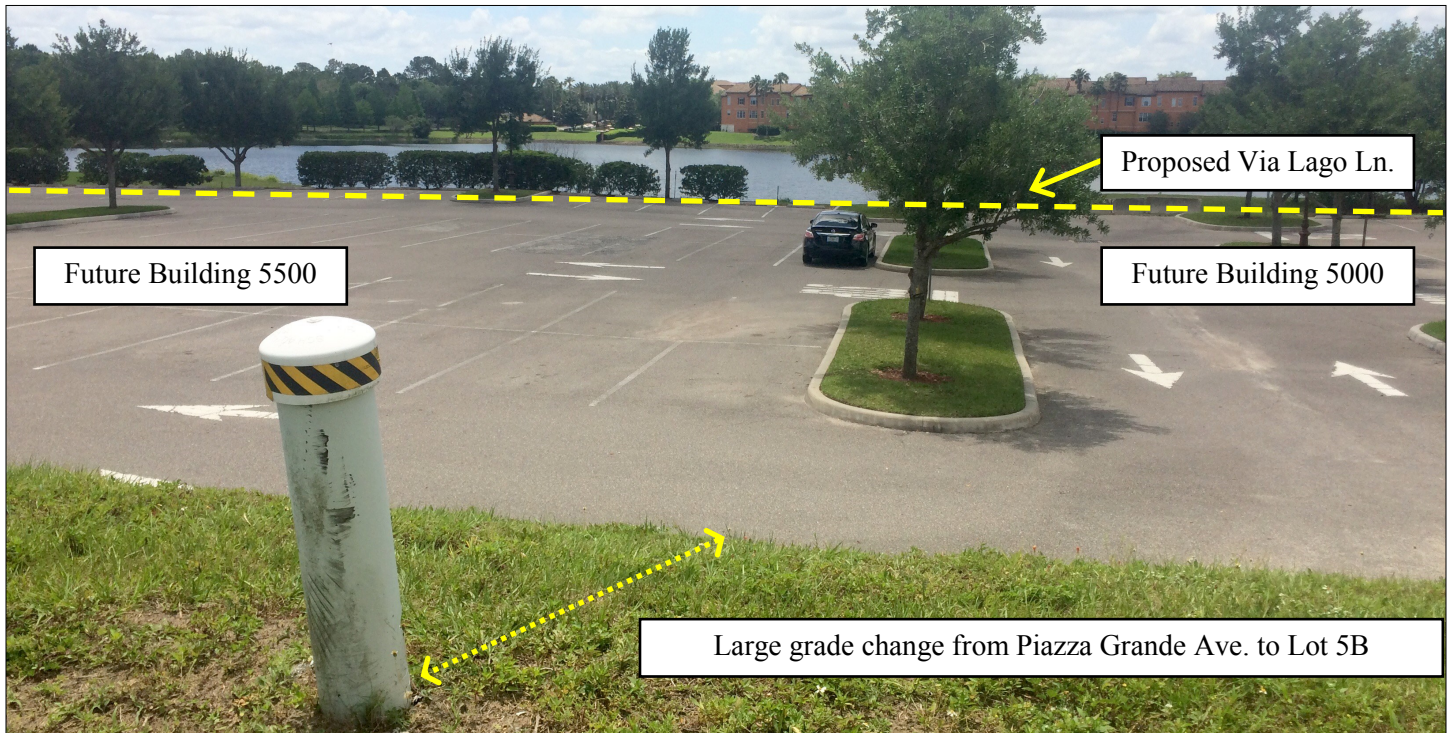
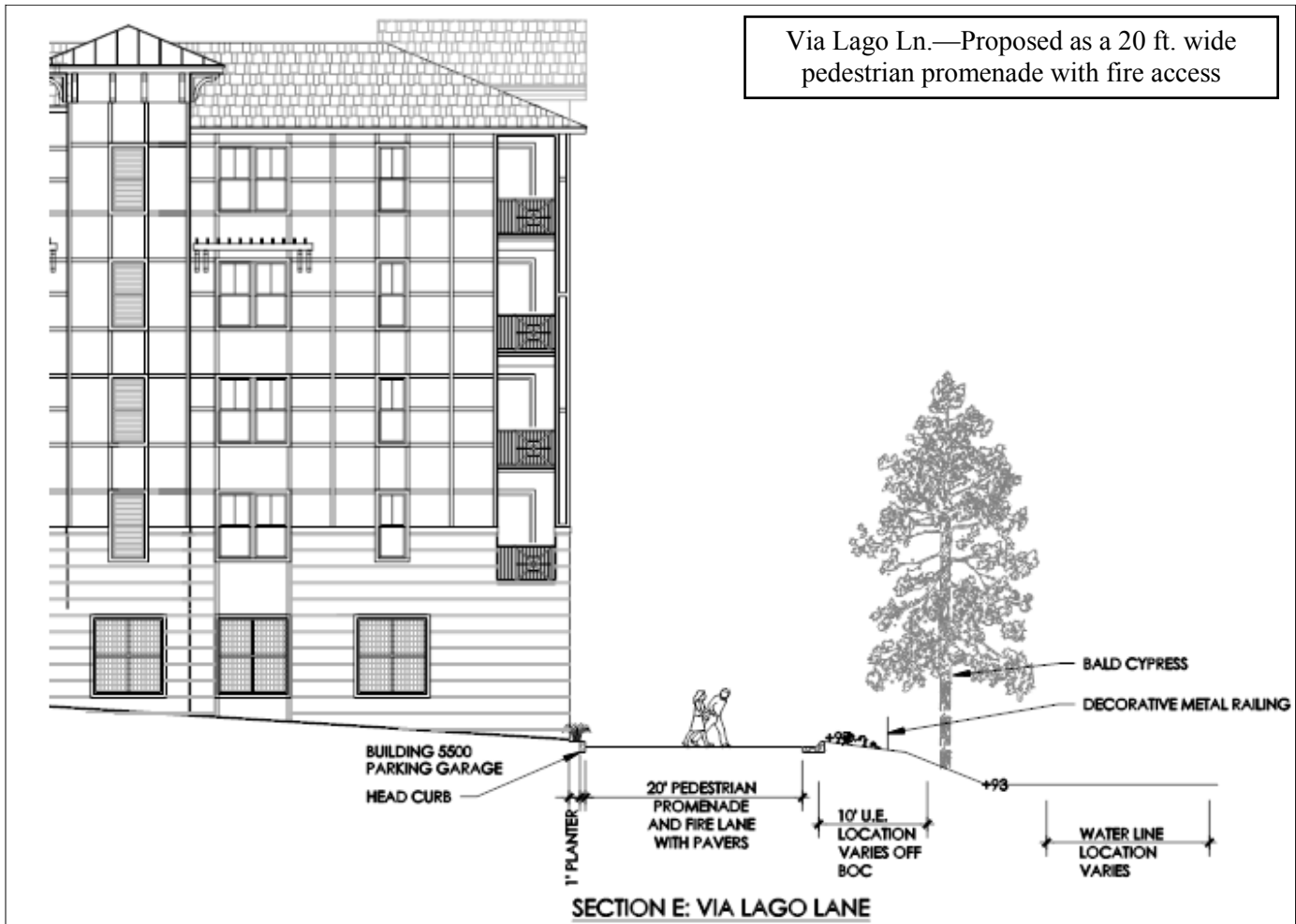
Existing retail east of Via Venzano Ave.



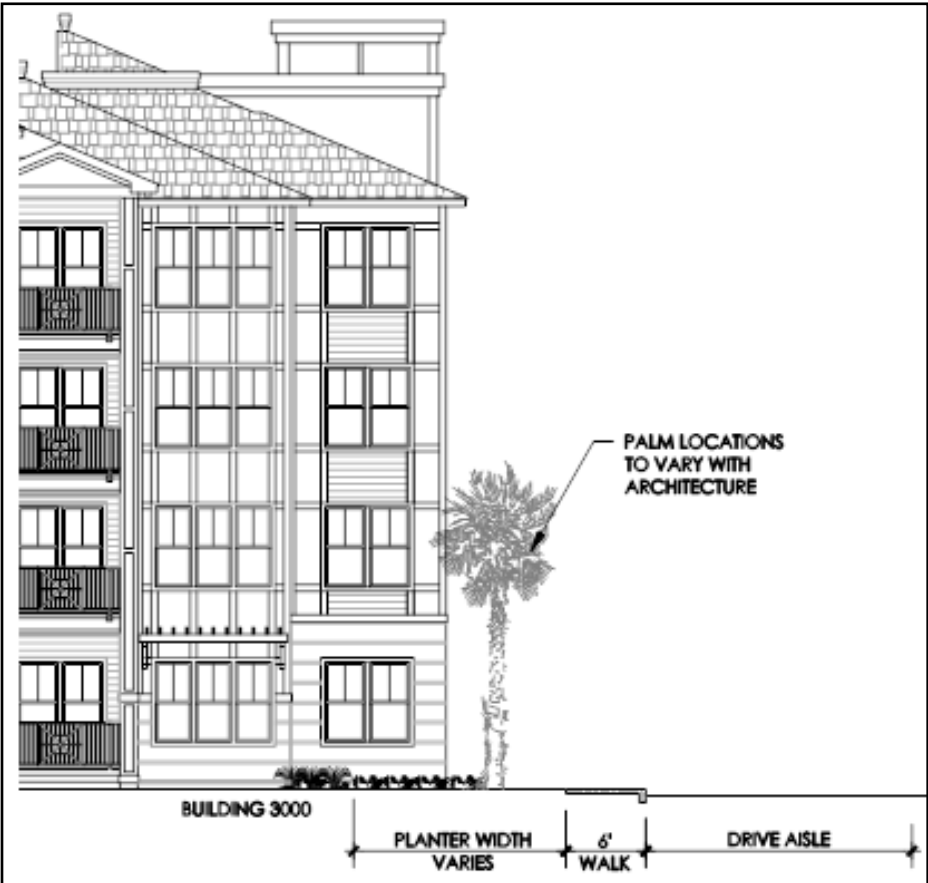
CROSS SECTION



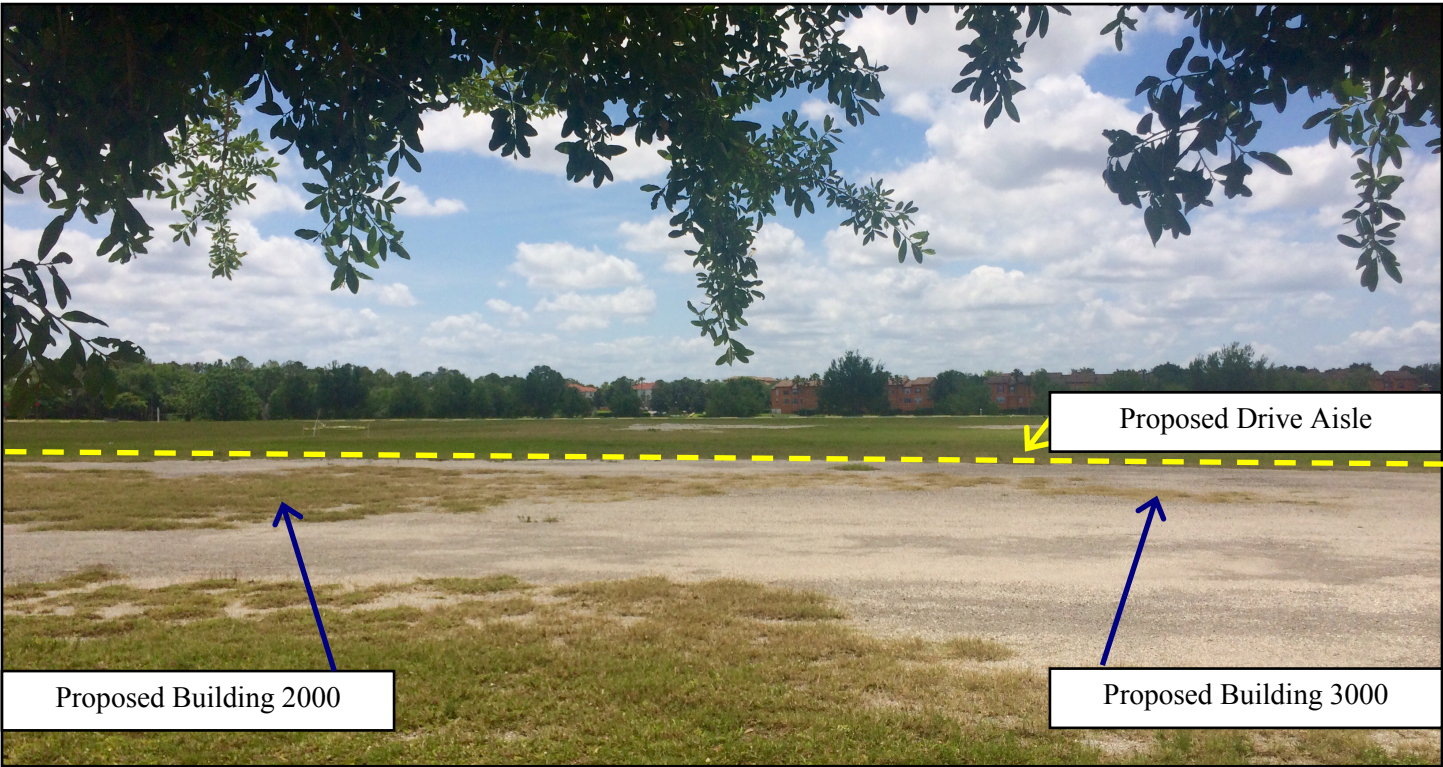
CROSS SECTION



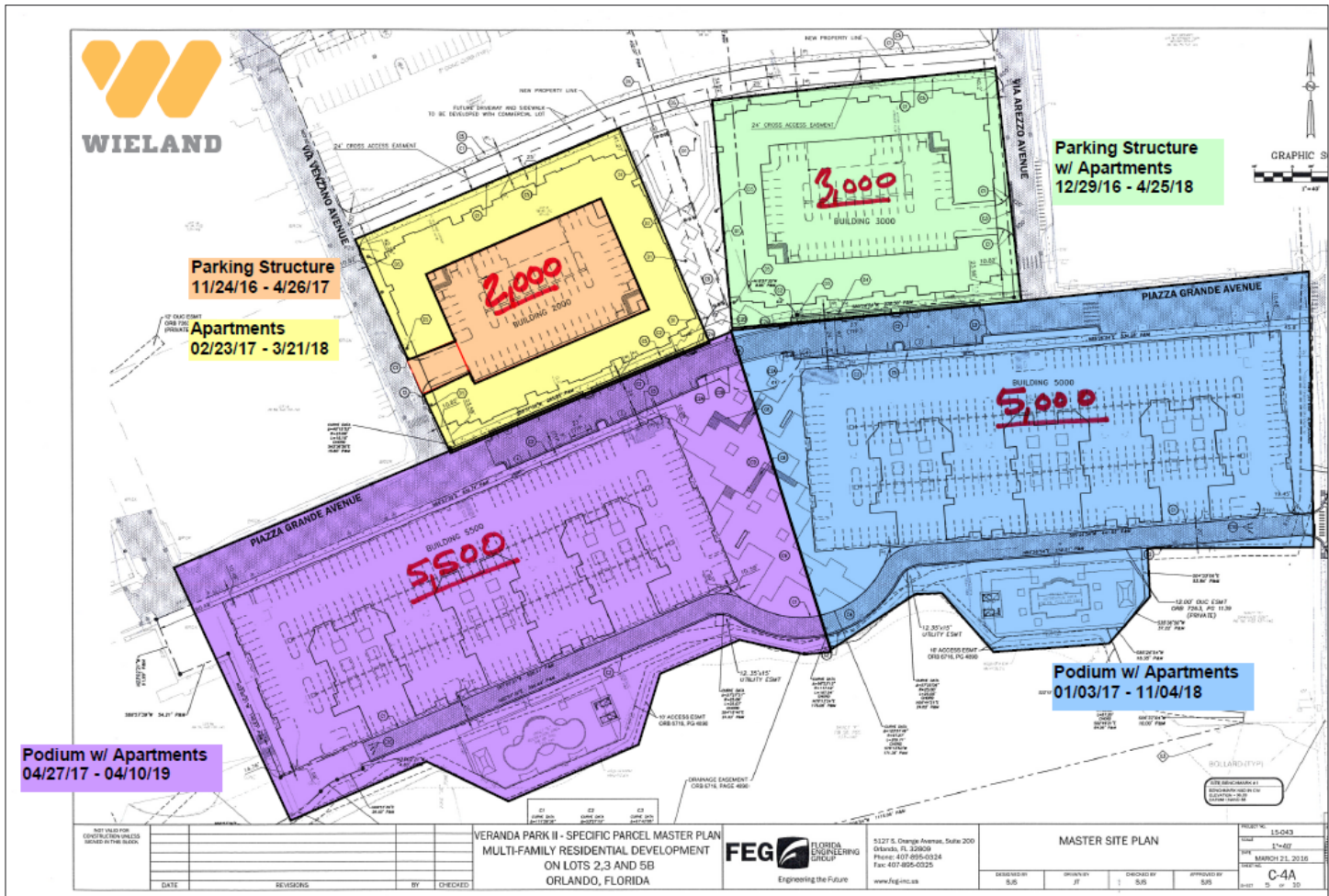
CROSS SECTION



East/West Drive Aisle proposed between Lot 3 (Building 2000 & 3000) and Lot 2 (future commercial)



PROPOSED PHASING PLAN & TIMELINE



Timeline

- November 2016 to April 2017: Building 2000 - Parking structure
- February 2017 to March 2018: Building 2000 - Apartments
- December 2016 to April 2018: Building 3000 - Parking structure with apartments
- January 2017 to November 2018: Building 5000 - Podium with apartments
- April 2017 to April 2019: Building 5500 - Podium with apartments

AERIAL PHOTO



FINDINGS

Subject to the conditions contained herein, the proposal is consistent with the requirements for approval of Master Plan applications contained in Section 65.335 of the Land Development Code (LDC):

1. The proposed use is consistent with the City's Growth Management Plan .
2. The proposed use is consistent with the purpose and intent of the AC-2 zoning district and all other requirements of the LDC.
3. The proposed use is be compatible with surrounding land uses and the general character of the area.
4. The necessary public facilities will be adequate to serve the proposed use, or will be provided by the applicant as a condition of this approval.

Staff Recommendation

Staff recommends approval of the subject master plan, subject to the conditions in this staff report.

CONDITIONS OF APPROVAL

Land Development

1. Land Use and Zoning. Except as provided herein, development of the property shall be consistent with the development standards of the AC-2 zoning district.
2. Variances and modifications. Zoning variances and modification of standards may be approved pursuant to the procedures set forth in Part 2J and Part 2F, Chapter 65, Orlando City Code, respectively. The planning official may also approve minor modifications and design modifications to fences, walls, landscaping, accessory structures, signs, and bufferyard requirements. Additionally, recognizing that development plans can change in small ways between the planning and permitting stages of development, the planning official may approve up to a 10% modification of any applicable numerical development standard if the planning official finds that the proposed modification is consistent with the applicable goals, objectives, and policies of the GMP, is compatible with nearby existing land uses, would not result in inadequate public facilities, and is otherwise consistent with the public health, safety, and welfare. When approving such a modification of a development standard, the planning official may impose one or more of the conditions of development provided at section 65.334, Orlando City Code, but such condition or conditions must be reasonably calculated to mitigate the identifiable land use impacts of the modified standard.
3. Permit Compliance. All plans submitted with the applicant's building permit application(s) must comply with the conditions of approval provided in the Municipal Planning Board staff report and any amendments to those conditions approved by the Municipal Planning Board or City Council. No building permit will be issued for this project until all conditions of approval are complied with and reflected in the plans submitted with the building permit application.
4. Regulations Subject to Code. Except as provided herein, the proposed project is subject to the conditions of this report and all codes and ordinances of the State of Florida, City of Orlando and all other applicable regulatory agencies. All other applicable state or federal permits must also be obtained before commencing development.
5. Master Plan Approval. Approval of the Master Plan by City Council shall grant the applicant authority to submit an application for final site plan approval for a building permit. Such application must be submitted within 24 months of approval or the Master Plan shall expire. However, the Planning Official may extend this time limit for one period of up to twelve months for good cause shown upon written application filed 30 days prior to the expiration date.
6. Signage. Signage shall comply with chapter 64 of the LDC. A master sign plan must be submitted via determination prior to pulling building permits.
7. Maximum Impervious Surface Ratio (ISR). The maximum ISR for the subject site shall be consistent with the AC-2 zoning designation, 0.90.
8. Parking. A minimum of 1,138 parking spaces must be provided for the four residential buildings proposed onsite with 642 dwelling units. In addition, 14 parking spaces are required for the 8 DUs added in building 4000. Also, 270 temporary parking spaces will be removed due to the new residential development. The applicant is responsible for the replacement of these parking spaces. Phase 2 will require a minimum of 133 parking spaces for 45,000 sq. ft. of commercial space. At the completion of Phase 1 there shall be a minimum of 2,275 parking spaces on-site.
9. Pedestrian Path. Provide stamped concrete, pavers, or an alternative material treatment with a smooth surface for all pedestrian crosswalks.
10. Phasing. Phase 1 shall follow the general timeline provided by the applicant on page 19 of this report. Some flexibility may be granted by the Zoning Official. The subject site shall host sufficient parking spaces at each phase of development. Phase 2 will be reviewed as an SPMP following the approval of this application.

Building Plan Review

Building Plan Review is not applicable to this case at this time. The building design will be reviewed for code compliance during the design development and construction documents phase.

Growth Management

Orange County Public Schools

Orange County Public Schools' concurrency letter, dated June 5, 2015, requires the applicant to enter into a Concurrency Mitigation Agreement for unvested multifamily dwelling units. An executed Concurrency Mitigation Agreement, between the applicant and OCPS, and addressing the 242 unvested dwelling units, shall be provided to the City prior to plat approval or final site plan (building permit).

Transportation Engineering

1. The entrances to parking lots under Buildings 5000 and 5500 shall be designed as 24 ft. wide driveways with curbs designed to guide vehicles into and out of the parking lots. Access to Via Lago Ln. shall be designed as a concrete curbcut off the parking lot driveways.
2. Means of emergency vehicle access shall be approved by OFD. Bollards are not an acceptable access prohibition for emergency

CONDITIONS OF APPROVAL(CONT.)

- access routes. The applicant has shown curbing and landscaping as a means of protecting the pedestrian street, but not limiting emergency access.
3. Sidewalks adjacent to parallel parking stalls shall be a minimum of 6 ft. wide.
 4. Sidewalks shall be constructed along Via Venzano Ave. and Via Arezzo Ave. adjacent to Buildings 2000 and 3000, respectively.
 7. Flush curb plazas open to vehicle traffic shall have detectable warning surfaces along the edge of the vehicular traveled way and bollards set back 4 ft. from the traveled way spaced no more than 10 ft. apart to prevent vehicles from driving on the pedestrian space. Planting beds or tree wells may be used as an alternative to bollards in the plaza area as long as the 4 ft. setback and max 10 ft. separation is maintained.
 8. Residential loading zones for each building shall be identified on the site plan. Loading zones and maneuvering areas shall not be located on public right-of-way.
 9. Dumpster locations and means of collection for each building shall be identified on the site plan.
 10. At all project entrances, clear sight distances for drivers and pedestrians shall not be blocked by signs, buildings, building columns, landscaping, or other visual impediments. No structure, fence, wall, or other visual impediment shall obstruct vision between 2 ft. and 8 ft. in height above street level. The street corner / driveway visibility area shall be shown and noted on construction plans and any future site plan submittals. The applicant shall design the site plan as necessary to comply with the Florida Greenbook and the FDOT Design Standards Index. Sight lines shall be provided on both site plans and landscape plans.
 11. For any construction work planned or required within a public right-of-way or City sidewalk easement adjacent to a public right-of-way (including but not limited to: irrigation, drainage, utility, cable, sidewalk, driveway, road construction/reconstruction or landscaping), the Owner/Applicant shall submit the following:
 - a. Maintenance of traffic plans (M.O.T.) (For more information/detailed requirements contact the Office of Special Events & Permits at 407-246-3704)
 - b. Roadway plans including paving, grading, pavement markings and signage (Contact the Permitting Transportation Engineering Reviewer at 407-246-3079 for details)
 - c. A copy of all required County and State permits (If permits are pending attach a copy of the application)

Transportation Planning

1. Number of Parking Spaces. Per the conditions of MPB case MPL2015-00007, the overall development is required to provide 2,408 parking spaces at final build-out. These parking spaces must be made available to serve the parking needs of the tenants and customers of the businesses and offices throughout the Veranda Park campus as well as for the proposed residential and commercial land uses.
As submitted, the proposed garages will provide an adequate number of spaces beyond the needs of the new development to satisfy this requirement.
2. Parking Garage Design. City Code allows for the use of Compact parking spaces in garages. The number constructed cannot exceed 15% of the total number of spaces within the facility. For Buildings 2000 and 3000, these maximums are 62 Compact spaces per garage. For Buildings 5000 and 5500, these maximums are 42 Compact spaces per garage. These may be distributed throughout the garage.
Stalls meeting ADA and Florida Accessibility Code requirements for size and quantity shall be included in the designs of all four parking garages and located near entrances or elevators.
Any stall created next to a lateral obstruction that will impede normal door swing, shall have the width increased by 12 in. This rule applies to Standard, Compact, and Low Turnover stalls.
3. Bicycle Parking. Per City Code Chapter 61, Part 3D, all developments are required to provide Bicycle Parking spaces for the use of patrons and employees. This project shall provide at least 11 long-term and 11 short-term bike parking spaces each in Buildings 5000 and 5500. 5 long-term spaces and 5 short-term spaces shall be provided in each of the other 2 buildings. If provided within the parking garage, all spaces may be grouped together or distributed as space allocation allows.
4. Construction Phasing. Due to the conditions from previous MPB approvals and the requirements of the Veranda Park framework master plan (MPL2015-00007), the applicant may not remove or restrict access to the surface parking lot on Lot 5B, until a comparable number of parking spaces are available for use by the campus via a temporary use parking lot built by the applicant in Lot 2 or Lot 3 or in one or more of the new garages.
5. Traffic Analysis. Condition from MPL2015-0007: Each parcel in this project will be required to submit turning movement counts and signal timing analysis in Synchro format for the 2 existing traffic signals at Westpointe Blvd. and Hiawasse Rd. and at Lake Debra Dr. and Hiawasse Rd. This analysis will include assessments of how any new traffic added as a result of development will impact the signal operation and timing. If more than one parcel is developed simultaneously, the analysis of all the parcels under development may be grouped into a single submittal to the City. Counts and Synchro files shall be submitted with any engineering or building permit application.

Transportation Impact Fees

1. Any new construction, change in use, addition, or redevelopment of a site or structure shall be subject to a review for Transportation Impact Fees. An estimated Transportation Impact Fee in the amount of \$1,622,334, based on the construction of 642-Multi-family residential units, will be due at the time of building permit issuance, subject to change upon final permit plan review.
2. For a copy of the complete ordinance or impact fee rate chart, you may reference our website at:<http://www.cityoforlando.net/planning/Transportation/ifees.htm>
3. The applicant shall comply with all applicable requirements of Chapter 59, the Concurrency Management Ordinance, to ensure that all public facilities and services are available concurrent with the proposed development, and that the potential impacts on public facilities and services are mitigated.

All new construction, changes in use, additions or redevelopment are required to submit a Concurrency Management application as a part of the building plan review process. A Concurrency Management application is available on the City's website: <http://www.cityoforlando.net/permits/forms/concurrency.htm>

Urban Design

1. An Appearance Review shall be required prior to application for building permits. All Urban Design conditions of approval shall be address in revised drawings prior to Appearance Review meeting.
2. Architectural elevations shall be generally consistent with those provided in the Master Plan application, except where otherwise directed herein. Significant deviation from those elevations shall require a master plan amendment. Minor changes may be approved by staff.
3. Primary pedestrian entries shall be clearly expressed and highlighted, and recessed or framed by a sheltering element such as an awning, arcade, porch or portico.
4. The base shall be at least 36 in. high and shall be of durable materials such as concrete, stone, or brick. Stucco will not be considered a durable material.
5. Ground floor glass shall be clear on all facades (minimum of 80% light transmittance), or low-e glass with a minimum light transmittance of 60%. All other glass shall meet a minimum 60% light transmittance. Darkly tinted or reflective glass shall be prohibited. Faux windows or spandrel glass shall not count towards transparency requirements.
6. All utilities, including street light poles, shall be kept out of the pedestrian path. Outdoor lighting must comply with Chapter 63, Part 2M, City Code. White light-emitting diode (LED) lamps are encouraged.
7. Dumpsters or trash compactors shall be either interior to the building, or shall be screened with solid walls to match the principal structure. Opaque decorative gates shall be installed to coordinate with principal structure. Landscape screen including low hedge and groundcover required to soften the view from the public ROW.
8. Any fencing shall be open, CPTED-approved fence, such as aluminum or wrought-iron picket fencing.
9. Provide stamped concrete, pavers, or an alternative material treatment with a smooth surface for all pedestrian crosswalks.
10. Any canopy trees installed in either the on-street parking planters of Piazza Grande Ave., Via Venzano Ave., Via Arezzo Ave. or in the public right-of-way of Lake Debra Dr., in planting areas that are less than 10 ft. in width (or on private property less than 5 ft. from public sidewalks) shall be installed with appropriate techniques to protect sidewalks, curbs, and infrastructure. Such techniques may include structural soil, pavement-supporting structures, root tunnels, and root barriers, and must be approved by the Appearance Review Official at time of permitting.
11. Tree clearing (excluding the clearing of invasive exotic species) shall not commence until full site and building development plans have been approved.
12. Street tree planting shall be completed along all roadway frontages abutting the site. The street trees shall be installed in accordance with City Code. In locations where installation of street trees within the street right-of-way or a City services easement is infeasible, the trees shall be installed on the site within 10 ft. of the right-of-way.

Fire

There are no objections to this request. Be advised that any new construction must adhere to the requirements of the Florida Fire Prevention Code, 2010 Edition, and The City of Orlando Fire Prevention Code.

TRC fire code review is preliminary in nature, and is intended to expose or prevent evident design deficiencies with State and City Fire Codes. The design will be reviewed in detail for State and City Fire Code compliance at the time of permit application.

OUC

Submit detailed water utility plans to Orlando Utilities Commission Development Services. See our Website for submittal information <http://www.ouc.com/business/water-services>.

Parks

Trees-Encroachment Contact Justin Garber, Parks Department at (407) 246-4047 or justin.garber@cityoforlando.net for a Tree Encroachment permit prior to encroaching within 6' of any existing 4 in. caliper or larger tree as part of Orlando Land Development

Code, Section 60.211.

Trees-Removal Contact Justin Garber, Parks Department at (407) 246-4047 or justin.garber@cityoforlando.net for a tree removal permit before removing any 4 in. caliper or larger trees.

Wastewater

Contact Vince Genco (vince.genco@cityoforlando.net) or David Breitrick (david.breitrick@cityoforlando.net) with the Wastewater Division to schedule a pre-application meeting to discuss the existing affected sanitary sewer system capacity and its ability to serve the wastewater flow generated by the proposed density and uses of the development. A capacity analysis shall be prepared for review by the Wastewater Division.

INFORMATIONAL COMMENTS

Transportation Planning

1. Commercial Plans Submittal:

The submitted construction plans shall include a site plan (to engineering scale) and show the following: property lines, legal description, curbs, street pavement edges, public sidewalks, loading/unloading areas, parking spaces, solid waste location, etc. (Please see the Commercial Plans Submittal Checklist: <http://www.cityoforlando.net/permits>). Reference Orlando Land Development Code (LDC), Chapters 60, 61, and 65, and the Engineering Standards Manual (ESM).

2. Major Thoroughfare Access:

The proposed access design shall meet Orlando Land Development Code (LDC), Chapter 61, Part 1 requirements in regards to, but not limited to the following:

- a. Spacing (driveway spacing from adjacent driveways)
- b. Joint-use driveways
- c. Corner clearance (driveway spacing from intersections)
- d. Driveway sight distance
- e. Unified cross-access and circulation
- f. Service vehicle access
- g. Tie-ins to abutting properties

3. Roadway Pavement Markings:

The installation of Pavement Markings and Traffic Control Devices (stop signs, signals, etc., as applicable to proposal) shall be shown per Manual of Uniform Traffic Control Devices (MUTCD) standards, and Orlando Engineering Standards Manual (ESM) Chapter 8, Section 8.01 requirements.

4. Handicap Ramps at Street Intersections:

Handicap (HC) ramps shall be constructed at the street intersection(s) and driveway connection(s) to comply with the Americans with Disability Act (ADA). Pedestrian ramps at street corners shall be designed to provide a separate ramp in each direction.

5. Driveways – Sight Distance:

At all project entrances, clear sight distances for drivers and pedestrians shall not be blocked by signs, buildings, building columns, landscaping, or other visual impediments. No structure, fence, wall, or other visual impediment shall obstruct vision between 2 ft. and 8 ft. in height above street level. The street corner / driveway visibility area shall be shown and noted on construction plans and any future site plan submittals. The applicant shall design the site plan as necessary to comply with the street corner visibility requirements (Chapter 60, Sections 60.141 through Sections 60.143) and the driveways and curb cuts requirements (Chapter 61, Sections 61.101 through Sections 61.107) of the Orlando Land Development Code (LDC).

6. Parking Lot Design:

The proposed parking lot must comply with Orlando Land Development Code (LDC) and Engineering Standards Manual (ESM) requirements. Refer to LDC, Chapter 61, Part 3, for parking and loading standards. The proposed parking lot shall be redesigned to meet the following requirements:

- a. Minimum acceptable angle of parking shall be 45 degrees.
- b. Standard automobile spaces shall be a minimum of 9 ft. wide by 18.5 ft. deep.
- c. Minimum acceptable aisle width for 2-way traffic flow shall be 24 ft.
- d. Parking lots shall be designed to eliminate the need for backing and maneuvering from or onto streets in order to maneuver out of parking spaces, maneuver between parking aisles or leave the lot.
- e. Compact spaces are not permitted in surface lots.
- f. Parallel parking stalls shall be a minimum of 9 ft. wide by 23 ft. deep.

7. Parking Lot Traffic Controls:

Parking lot traffic control shall be shown per Manual of Uniform Traffic Control Devices (MUTCD) standards (stop signs, stop bars/lines, "Do Not Enter" signs, and pavement arrows) at internal driving aisle intersections. Show 30 in. stop signs and 24 in. stop bars/lines at exits from property. Stop signs and stop bars/lines shall be located in-line and 4 ft. behind sidewalks/

INFORMATIONAL COMMENTS (CONT.)

crosswalks.

8. On-Site Sidewalk System:

A paved pedestrian/handicap connection and/or sidewalk shall be provided from the public sidewalks to the main building entrances.

9. Pedestrian Crossings:

The main pedestrian crossing areas between the buildings and the parking areas shall be clearly marked with pavement markings and maintained at all times in accordance with Manual of Uniform Traffic Control Devices (MUTCD) standards.

10. Door Openings:

Doors (and any pedestrian entrances) that lead directly from the sidewalk shall be recessed so the doors do not swing open into the sidewalk area.

11. Landscaping:

The location of any existing, required, and proposed parking lot landscaping, including trees, hedges, and walls/fences, shall be shown on the site plan and meet the requirements of the Orlando Land Development Code (LDC), Chapters 60 and 61.

12. Dumpster/Compactor:

Garbage containers shall be located to allow direct access without requiring excessive maneuvering or backing up more than 50 ft.

Permitting Services

1. The Office of Permitting Services recommends approval of the proposed Master Plan, subject to the above listed conditions and requirements.
2. Street Tree Fund: The Orlando City Council approved a Resolution at the January 27, 1997 City Council Meeting. Section 61.226 of the City's Land Development Code provides for a Street Tree Trust Fund. The developer is responsible for the installation of street trees prior to final of the building permit. The Street Tree specifications are 12—14 ft. height of canopy tree.
3. In accordance with City Code Section 28.6 (f) 1-3, the Office of Permitting Services is authorized to make a determination of approval/disapproval of refuse container sites. Approval/disapproval of the use of commercial hand pick-up of refuse from any non-residential entity shall be determined solely by the Refuse Collection Bureau Staff.
4. At the time of development, the owner/developer is required to pay an on-site inspection fee that is a percentage of the cost of the on-site improvements, excluding the building, in accordance with City Land Development Code, Section 65.604.
5. The owner/developer is required to design and construct an on-site storm water system in accordance with the Orlando Urban Storm Water Management Manual and the approved Master Drainage Plan. Approval from South Florida Water Management District is required. The system is to be privately owned and maintained.
6. Construction activities including clearing, grading and excavating activities shall obtain an Environmental Protection Agency (EPA) National Pollution Discharge Elimination System (NPDES) permit, except: Operations that result in the disturbance of one acre total land area which are not part of a larger common plan of development or sale.
7. The City Council Adopted the Engineering Standards Manual (ESM), Fourth Edition on March 9, 2009. All plans must conform to the ESM and all construction must be accomplished in accordance to the ESM.
8. See Chapter 64 Orlando Land Development Code for sign requirements and regulations. Separate permit applications are required for signs.
9. As per Section 61.225 of the Land Development Code, a 5 ft. wide concrete sidewalk is required along all dedicated rights-of-way. Any existing sidewalk damaged or broken is to be repaired.
10. All new construction, change in use, additions, or redevelopments are required to submit a Concurrency Management application as a part of the building plan review process.

Engineering/Zoning

1. At the time of site development the contractor shall provide certification for the installation of the HDPE, also coordination of the redirect of the existing 10" storm pipe from the adjacent site (existing to be diverted).
2. All future elevation shown on a boundary/topographic survey shall use the North American Vertical Datum of 1988 (NAVD 88).
3. A valid survey is required to be submitted with the Final Plat and Engineering Plans that is signed and sealed by a license PSM. The survey provided with this application is invalid.

Police

The Orlando Police Department has reviewed the plans for Veranda Park II Apartments located at 2201 S. Hiawasse Rd., utilizing CPTED (Crime Prevention Through Environmental Design) principles. CPTED emphasizes the proper design and effective use of a created environment to reduce crime and enhance the quality of life. There are four overlapping strategies in CPTED that apply to any development: Natural Surveillance, Natural Access Control, Territorial Reinforcement and Target Hardening.

CONTACT INFORMATION

Growth Management

For questions regarding Growth Management plan review, please contact Wes Shaffer at (407) 246-3792 or thomas.shaffer@cityoforlando.net.

Land Development

For questions regarding Land Development review, please contact Katy Magruder at (407) 246-3355 or kathleen.magruder@cityoforlando.net.

Urban Design

For questions regarding Urban Design Review, please contact Terrence Miller, at 407.246.3292 or Terrence.miller@cityoforlando.net

Transportation

For questions regarding Transportation Planning plan review, please contact John Rhoades at 407-246-2293 or john.rhoades@cityoforlando.net

For questions regarding Transportation Engineering plan review, please contact Lauren Torres at 407-246-3220 or lauren.torres@cityoforlando.net

For questions and information regarding Transportation Impact Fee Rates you may contact Nancy Ottini at (407)246-3529 or nancy.jurus-ottini@cityoforlando.net

Police

For questions regarding the Orlando Police Department plan review, please contact Audra Nordaby at 407.246.2454 or Audra.Nordaby@cityoforlando.net.

Building

For questions regarding Building Plan Review issues contact Don Fields at (407) 246-2654 or don.fields@cityoforlando.net.

REVIEW/APPROVAL PROCESS—NEXT STEPS

1. MPB minutes scheduled for review and approval by City Council.
2. Final site plan review via determination will be required prior to the issuance of building permits.
3. Submit for building permits.