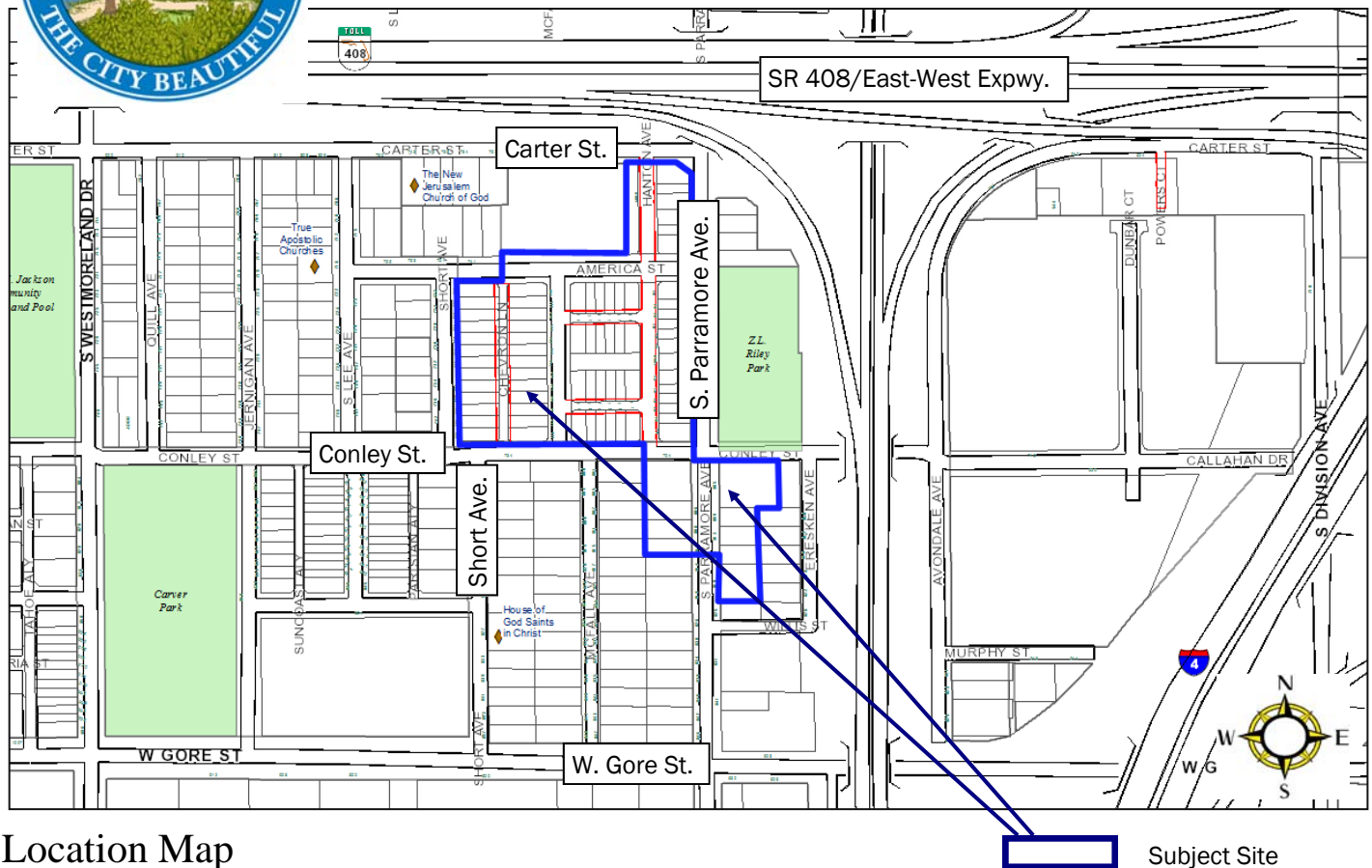




PARRAMORE OAKS FRAMEWORK PD  
(PLANNED DEVELOPMENT)  
744 S. PARRAMORE AVE.



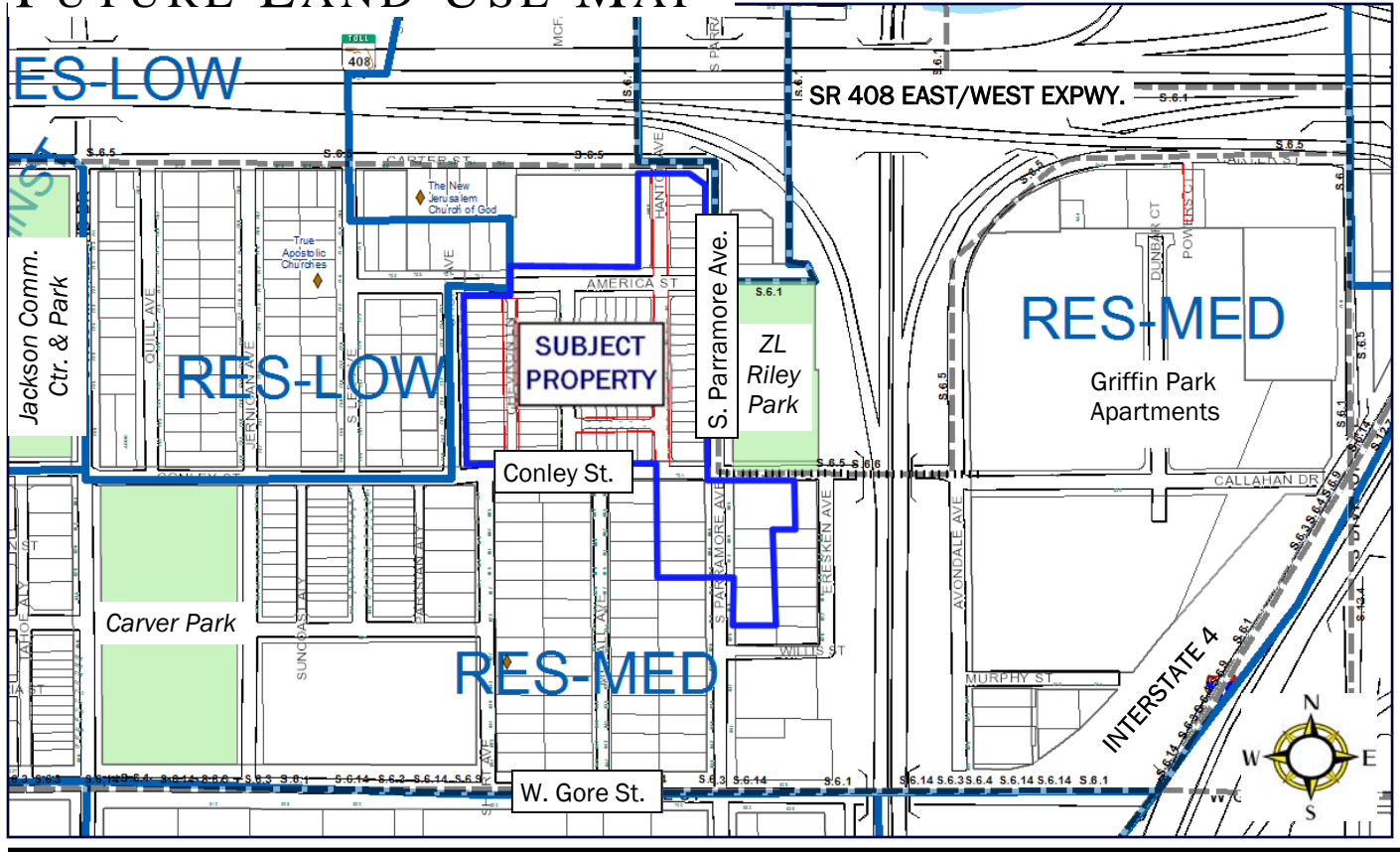
Location Map

Subject Site

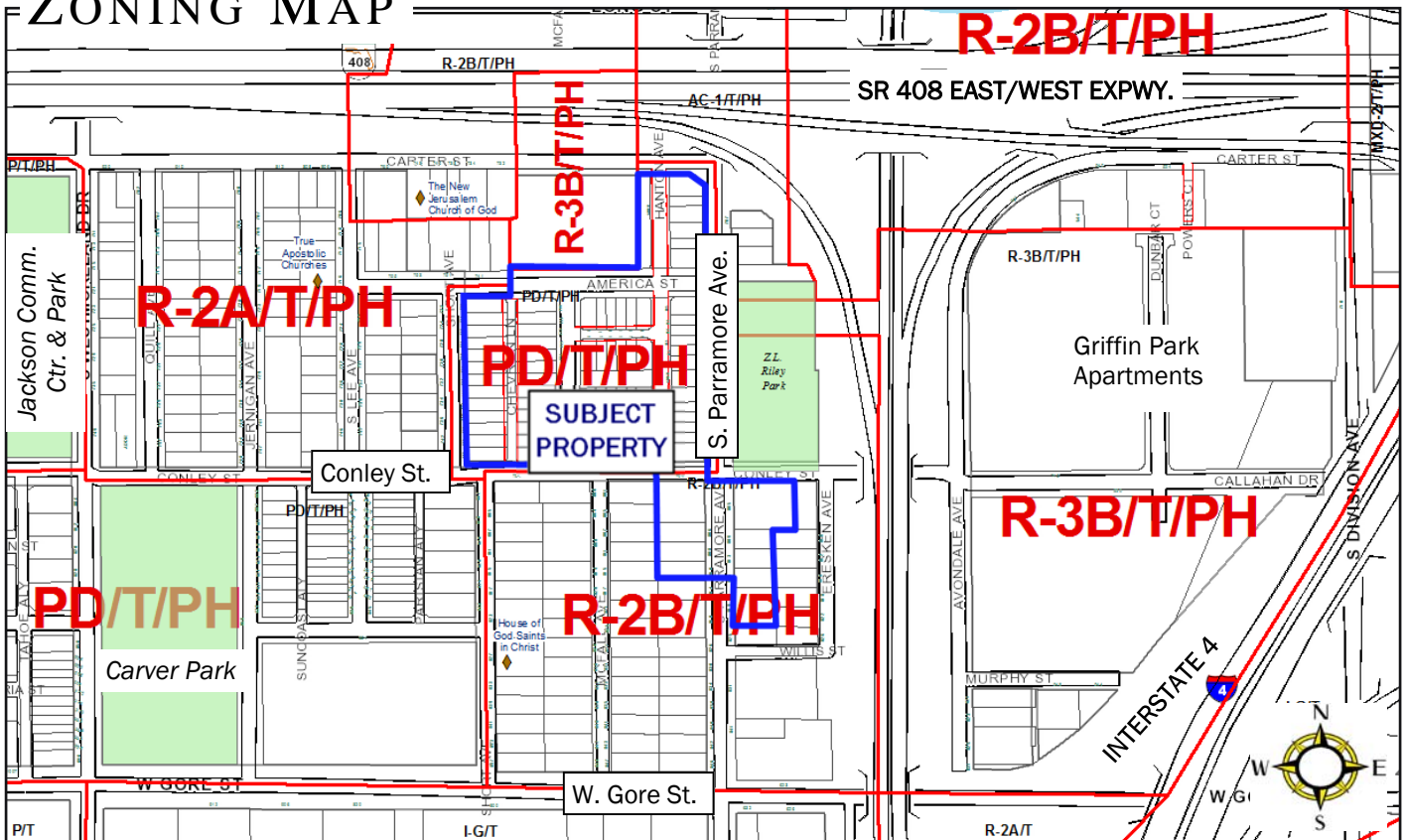
SUMMARY

<p><b>Applicant</b> Richard Cavaliere Invictus Development</p> <p><b>Owner</b> Community Redevelopment Agency (CRA) of Orlando &amp; City of Orlando</p> <p><b>Project Planner</b> Jim Burnett, AICP</p> <p><b>Updated:</b> October 11, 2016</p>	<p><b>Property Location:</b> 744 S. Parramore Ave. (multiple parcels, generally between Carter St. on the north and Willis St. on the south, and between Eresken Ave. on the east and Short Ave. on the west)(±6.34 acres, District 5).</p> <p><b>Applicant's Request:</b> The applicant is requesting a new framework Planned Development (PD) to establish a phased 178-unit multi-family and 33 townhouse development in the Parramore neighborhood. An affordable housing bonus of 3.2 dwelling units per acre (du/ac) is needed, which will result in a net density of 33.2 du/ac.</p>	<p>The property is designated as Residential Medium Intensity on the City's Future Land Use Map, and is currently zoned PD (former Wells Landing PD) and R-2B/T/PH (PD will have default zoning of R-3C/T/PH).</p> <p><b>Staff's Recommendation:</b> Approval of the framework PD request, subject to the conditions in this staff report.</p> <p><b>Public Comment:</b> Courtesy notices were mailed to property owners within 400 ft. of the property the week of October 3, 2016. A community meeting was held on 9/26/2016, with 30 persons attending. As of the mail-out of the staff report, staff has not received any comments relative to the PD request.</p>
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# FUTURE LAND USE MAP



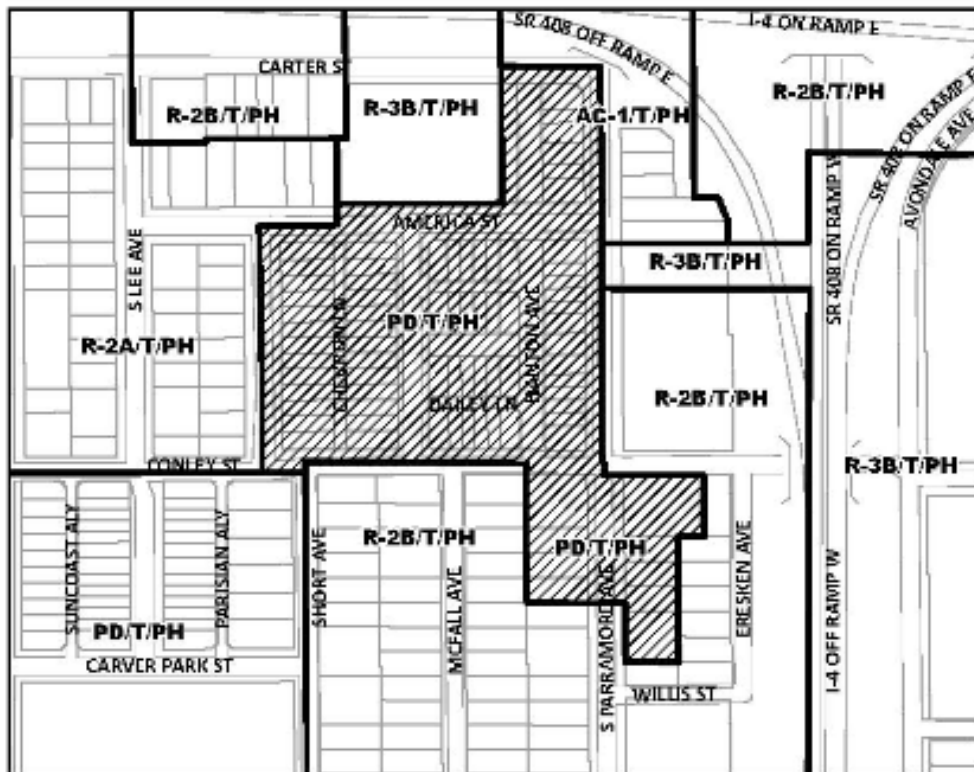
# ZONING MAP



# BEFORE AND AFTER ZONING MAPS



Zoning - Existing ZON2016-00024



Zoning - Proposed ZON2016-00024



# PROJECT ANALYSIS

## Project Description

The applicant is requesting approval of a new framework Planned Development (PD) to establish a phased 178-unit 3 – and 4-story multi-family and 33 2-story townhouse development in the Parramore neighborhood. An affordable housing bonus of 12.2 dwelling units per acre (du/ac) is needed, which will result in a net density of 33.2 du/ac.

The property is designated as Residential Medium Intensity on the City's Future Land Use Map, and is zoned PD (the former Wells Landing PD was never constructed) and R-2B/T/PH.

## Previous Actions:

- 1915: Properties at southeast corner of Conley St. and S. Parramore Ave. originally platted as part of the McElroy & Boone Addition to Orlando.
- 1920: Properties at southwest corner of Conley St. and S. Parramore Ave. originally platted as part of the James Steversons Subdivision.
- 1923: Properties at southeast corner of Short Ave. and America St. originally platted as part of 1st Addition to Western Terrace Subdivision.
- 1965: Large portion of (Wells Landing) property originally platted as Parramore Village Subdivision (including America and Weaver Cts); Parramore Village PUD (Planned Unit Development) constructed, consisting of 75 single-family attached townhomes).
- 1966: Portions of the property replatted as Parramore Village 1st and 2nd Replats.
- 1990: City determines area to be "blighted," establishes Community Redevelopment Agency (CRA).
- 1995: Property at southwest corner of Carter St. and S. Parramore Ave. platted as part of Suncharm Subdivision.
- 2002: City establishes Parramore Village project area as prelude to redevelopment of the property.
- 2004: Parramore Village demolished.
- 2009: Weaver Ct. and portion of America St. abandoned (ABN2008-00006, City Doc. #0906221103); former Parramore Village property rezoned to PD (ZON2008-00040, City Doc. #0906221102) for proposed Wells Landing townhome and single-family home development; subject properties replatted as Wells Landing Subdivision.
- 2015: Property never developed, property transferred back to CRA ownership; solicitation submitted for redevelopment proposals, applications received late 2015.

## Project Context

The proposed development will be located on several City- and CRA-owned lots between S. Parramore and Short Aves. and on the south side of Conley St., on the east and west sides of S. Parramore Ave. All essential services such as water and sewer, are already at the sites. Both the proposed multi-family and townhouse developments are allowed uses in the existing R-2B/T/PH and default R-3C/T/PH zoning but the proposed overall development plan requires a new framework Planned Development to ensure superior design for what the City hopes will be a signature development. Future Land Use designations, zoning and adjacent uses are shown in Table 1 at the top of the following page.

## Planned Development Criteria (LDC Chapter 58 Part 2Q & Chapter 65 Section 65.290)

The Municipal Planning Board and City Council shall consider the following factors in their review of Planned Development applications:

1. Purpose and Intent. The purpose and intent of the use and all other requirements of LDC.
2. Growth Management Plan (GMP). The consistency of the proposal with all applicable policies of the City's adopted GMP.
3. Use and District Requirements. The proposal must conform to all applicable performance and design regulations of LDC Chapter 58, 60, 61, and 62.
4. Public Facilities and Services. Will necessary public facilities (both on- and off-site), such as transportation, sanitation, water, sewer, drainage, emergency services, education, recreation, etc. be adequate to serve the proposed use.

## Conformance with Growth Management Plan and Zoning

The property is designated Residential Medium Intensity (RES-MED) on the City's Future land Use Map and is zoned PD (the former Wells Landing PD was never constructed) and R-2B/T/PH. As noted above under Project Context, the proposed development would ordinarily require a Master Plan but is being done as a PD to ensure superior design and also because an affordable housing density bonus is being requested. Property development standards are

provided in Table 2 below.

	<b>Future Land Use</b>	<b>Zoning</b>	<b>Adjacent Use</b>
North	(Across America & Carter Sts.) Residential Low Intensity (RES-LOW), Residential Medium Intensity (RES-MED) and Community Activity Center (COMM-AC)	R-2A/T/PH (One- and Two-Family Residential, Traditional City Overlay, Parramore Heritage Overlay); R-3B/T/PH (Medium Intensity Residential Development, same overlays as above); AC-1/T/PH (Community Activity Center, same overlays as above) and R-2B/T/PH (One- to Five-Family Residential, same overlays as above)	Single-Family Homes, Richard Allen Apartments, SR 408/East-West Expwy and Z.L. Riley Park
East	(Across S. Parramore Ave., et al) COMM-AC & RES-MED	AC-1/T/PH, R-3B/T/PH & R-2B/T/PH	Retail Building, Z.L. Riley Park & (south of Conley St.) Single-Family Homes & Duplexes
South	(Across Conley St.) RES-MED	PD/T/PH (Carver Park Planned Development, same overlays as above) & R-2B/T/PH	Single-Family Homes & Duplexes
West	(Across Short & S. Parramore Aves.) RES-MED & RES-LOW	R-3B/T/PH, R-2A/T/PH & R-2B/T/PH	Richard Allen Apartments, Single-Family Homes and Duplexes

Phase	Proposed Use	Area (acres)	Setbacks - Minimum / <u>Proposed (P)</u> (ft.)						ISR (%) (max/prop)	Bld. Ht. (ft.)
			Units	Front	Street Side	Side	Rear	Perimeter		
1	Apartments	4.43	101	(E) 25 / 0	25 / 5	20 / 20	35 / 22	none	80 / ≤80	45 / 55
	Townhomes		19	**20 / 4	15 / 10	10 / 38	20 / 37	10 / 37+		25
2	Apartments	1.91	77	(S/W) 25 / 5	25 / 2	20 / 16	35 / 35+	none		45 / 55
	Townhomes		14	(W) 20 / 7.5	15 / 4	10 / 7.5	20 / 25	10 / 7.5+		25

\*PD needed for zoning setback & building height relief and to allow density bonus of 3.2 du/ac (for net density of 33.2 du/ac);  
 \*\* see site plan for frontages.

Affordable Housing Intensity Bonus

Land Development Code (LDC) Section 58.1133 states that “intensity standards for permitted or conditional uses in certain zoning districts may be increased in exchange for a contribution by the developer to the City of Orlando Trust Fund for Low and Very-Low Income Housing, or for an on-site housing alternative.” As previously noted, the new framework PD will have R-3C/T/PH as the default zoning, which has a maximum density of 30 dwelling units per acre (du/ac). The property has Residential Medium Intensity future land use, which has the same maximum density allowance. Thus, a density bonus is needed to achieve the additional 3.2 du/ac, for a total density of 33.2 du/ac.

The affordable housing intensity bonus requires a neighborhood compatibility analysis, which was provided as part of the recent Parramore Comprehensive Neighborhood Plan (Growth Management Plan, Future Land Use Subarea Policy S.6.14). Much of the surrounding Parramore/Holden neighborhood is filled with a mix of single-family homes, duplexes and apartment buildings, so the proposed project should blend in well with the existing residential neighborhood. Thus, the proposed density and residential uses are compatible with the surrounding area and consistent with the adopted Residential Medium Intensity future land use designation via the framework PD.

**Conformance with the LDC**

The setback provisions of LDC Section 58.572 apply for the multi-family portion of the PD (see Table 2 above),

while the provisions of LDC Section 58.513 apply for the proposed townhouses within the PD. The proposed development will be urban in nature, so front and street side setbacks will be much less than what is typically allowed. Proposed side and rear setbacks are closer to or exceeding the minimums required by Code. Thus, the PD will provide reduced front and street-side setbacks, and slightly taller multi-family buildings, within the proposed framework PD.

**Phasing/Urban Design** - The apartment buildings will be 3 and 4-stories tall with rear/interior parking spaces tucked under the 2nd floor units. 101 multi-family units and 19 townhouse units are proposed for Phase 1, with another 77 multi-family units and 14 more townhouse units slated for Phase 2. Per Code, the overall site impervious surface ratio (ISR) under the default R-3C/T/PH zoning cannot exceed 80%, and it's anticipated that the PD will be developed below that maximum threshold.

**Transportation**

The proposed PD will enjoy access from S. Parramore Ave., Conley St., and from Short Ave. to the west. The site abuts residential apartments to the north, so cross-access is not available in that direction. Pedestrian access will be provided throughout the development to existing and new sidewalks on all the above streets.

**Table 3 - Parking** (per LDC Section 61.322, Fig. 27)

Proposed Apartments	Unit Count	Parking Ratio <i>spaces/dwelling unit</i>	Required Spaces	Proposed Spaces
1-bedroom	36	1.5 spaces	54	210 + 12 on-street spaces = <b>222</b>
2-bedroom	104	1.75 spaces	182	
3-bedroom	38	2 spaces	76	
		Total	<b>312</b>	
Townhouses	33	2 spaces	66	<b>66</b>
Total	211		378	<b>288</b>

**Parking** - The site will be parked as illustrated in Table 3 at right. An adequate number of spaces are being provided for residents, given that the site is heavily reliant on City transit services and other modes of transportation. City Code requires short- and long- term bike parking spaces for the proposed use. Said spaces should be located near the building entrances or in a sheltered area elsewhere on the site.

**Transit** - The site is served by LYNX bus service, route #8 West Oak Ridge/International Dr., and neighborhood residents have historically walked and used bicycles, so a parking reduction of 90 spaces is also being requested via the PD. The main rear surface parking lot for the multi-family use is bisected by the phasing line, which will result in a 28 space deficit for City Code minimum parking requirements for Phase 1. If parking demand within Phase I becomes problematic as determined by City Code Enforcement and the Zoning Official, the property owner/developer will be required to construct the 28 parking spaces to the west of the current phase line, with the parking situation re-evaluated prior to the commencement of Phase 2 construction.

**Transportation Impact Fees** - Applicable Transportation Impact Fees will be determined at time of Specific Parcel Master Plan review for each phase. Said impact fees will be due at the time of building permit issuance, subject to change upon final permit plan review.

**Urban Design**

**Signage** - Maximum sign area for multi-family developments is limited to 18 sq. ft. per sign face, per LDC Section 64.257, with additional interior sign area of 36 sq. ft. allowed per sign. The building elevations show proposed vertical projecting signs higher than 30 ft. in elevation, making them high-rise signs. Per LDC Section 64.246, high-rise sign area is based on the gross area of a building, not to exceed 800 sq. ft. of high-rise sign area and to be evenly split into not more than two (2) equal area projecting signs. High-rise signs can be electronically lit with any color, but must glow white at night.

An estimate of the allowed high-rise sign area cannot be made at this time. A Sign Master Plan must be approved via Planning Official Determination prior to any signs being erected on the site. All signage must be permitted prior to fab-

**Table 4 - Buffers & Landscaping (Multi-Family & Townhomes)**

	<i>Front - N (Carter St. / SR408)</i>	<i>Street Side - E (S. Parramore Ave.)</i>	<i>Side - W (Apts. &amp; Short Ave.)</i>	<i>Rear - S (s. of Conley St.)</i>
Minimum Required	5 ft. (building setback / buffer)	No buffer needed for apartments; Bufferyard A for THs	7.5 ft. buffer next to aptmts; 4 ft. building setback for THs on Short Ave.	Bufferyard A for THs south of Conley St.
Proposed	5 ft. between apartments and front lot line	5 ft. with trees but no buffer wall (next to THs)	7.5 ft. buffer (next to apartments); 7 ft. buffer w. trees west of THs	7.5 ft. buffer (south of TH's south of Conley)



rication and installation.

**Landscaping & Buffers** - Required and proposed landscaping/buffers is provided in Table 4 at right. A preliminary landscape plan was provided with the framework PD request.

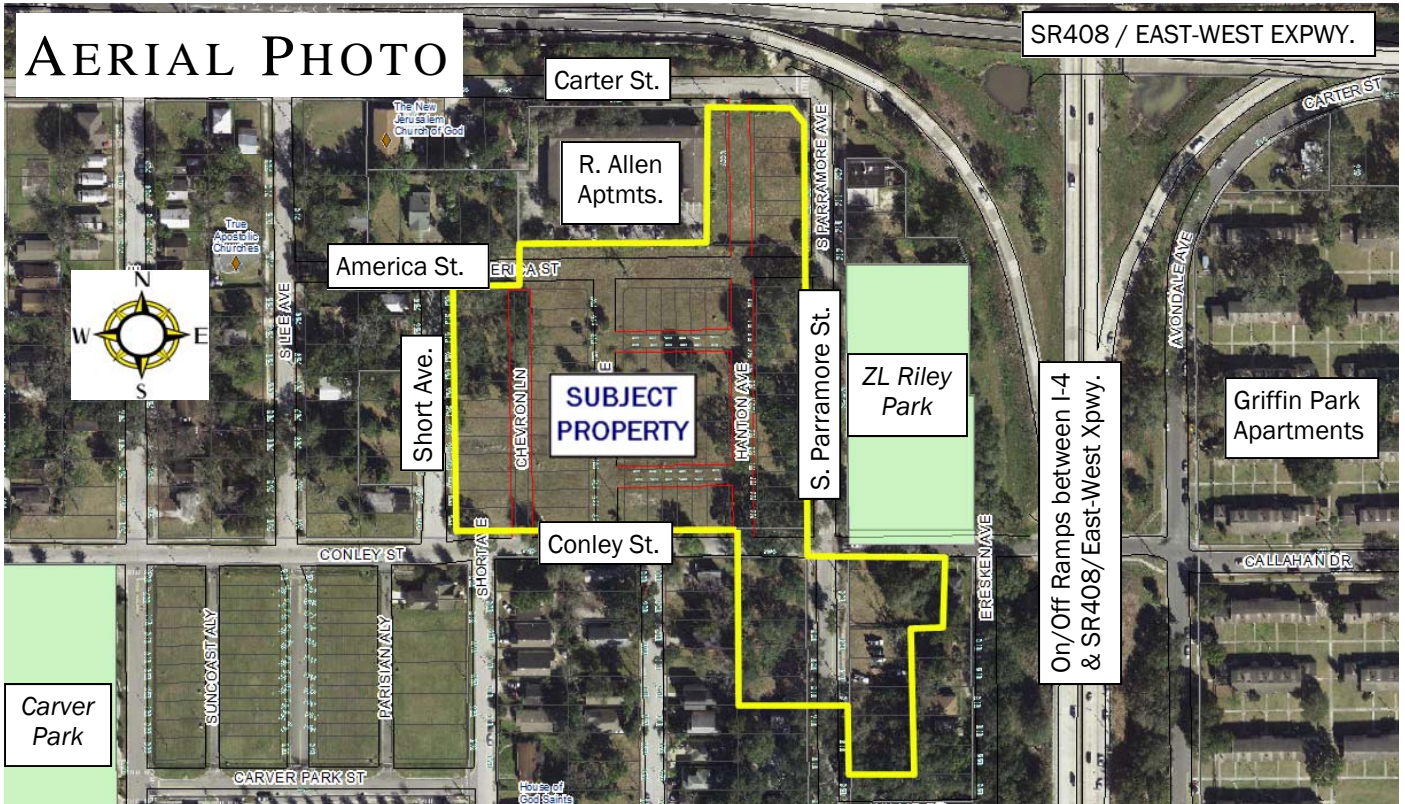
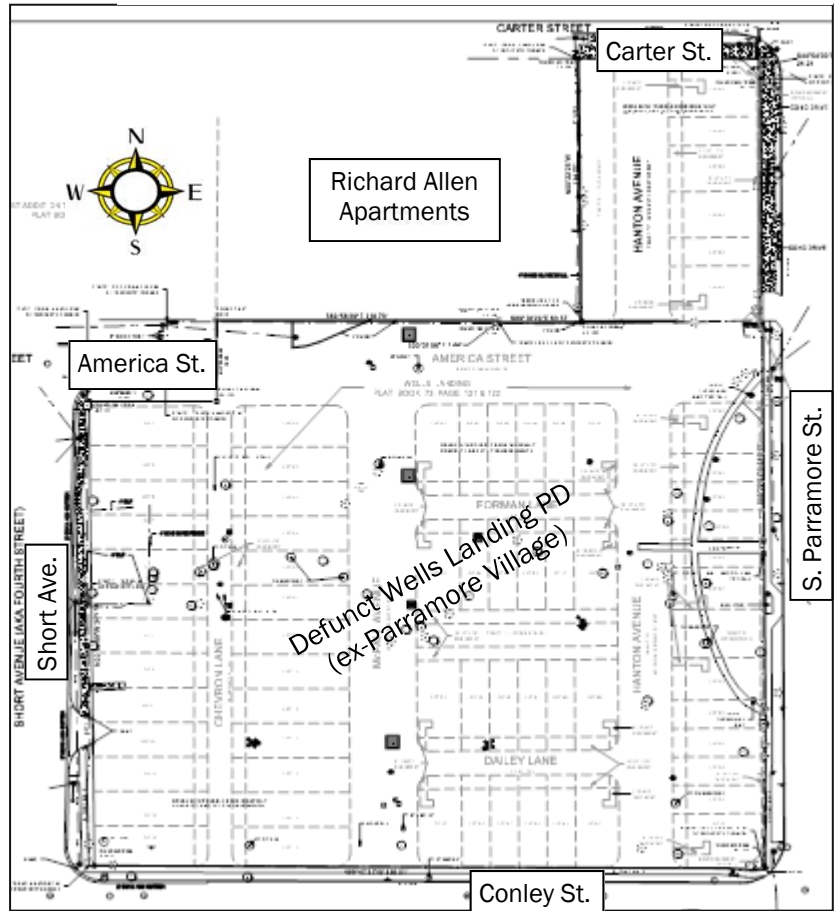
**Solid Waste** - A compactor and recycling bins will be located in a walled and landscaped area on the north, off the rear entrance from America St., to afford solid waste trucks clear entry and exit. The townhome units will have individual residential pickup service.

**Stormwater** - Stormwater will be routed to the Carver Park master pond 500 ft. to the west. A sewer capacity analysis needs to be prepared by the applicant prior to vertical construction permits being requested.

**School Impacts**

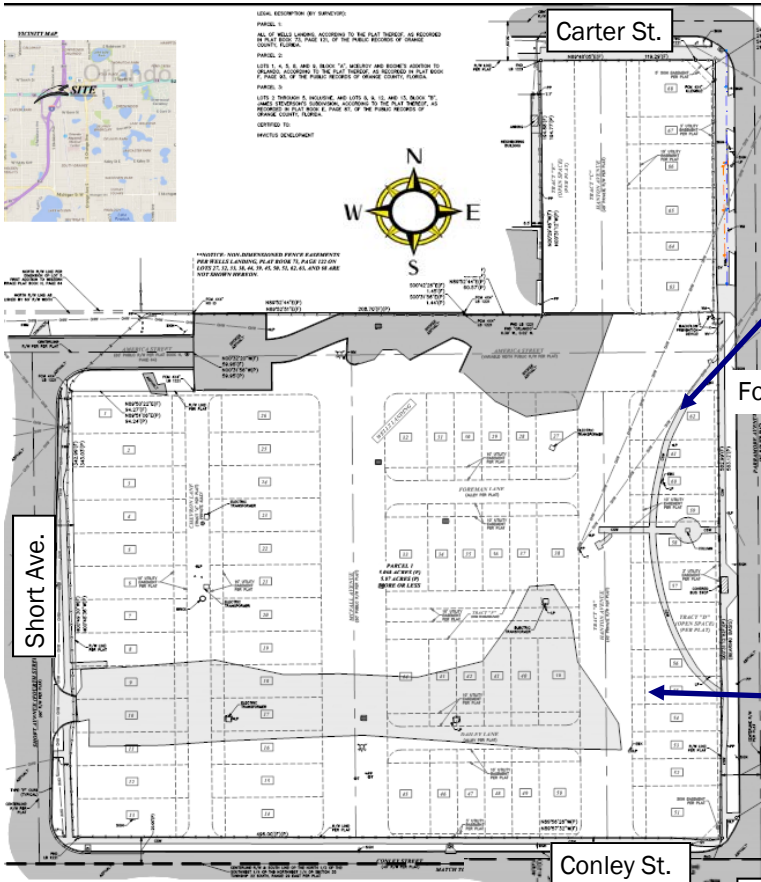
The project results in 26 single family units and 61 multifamily units, or 87 dwelling units being vested (from the previous Wells Landing PD and additional land being added to the new PD). A capacity agreement will need to be approved prior to the 2nd reading of the PD Ordinance. A concurrency agreement must be approved by OCPS and provided to the project planner prior to the recording of the plat. School impact fees may also be required for the overall 211-unit development.

# LIMITS OF EXISTING PD





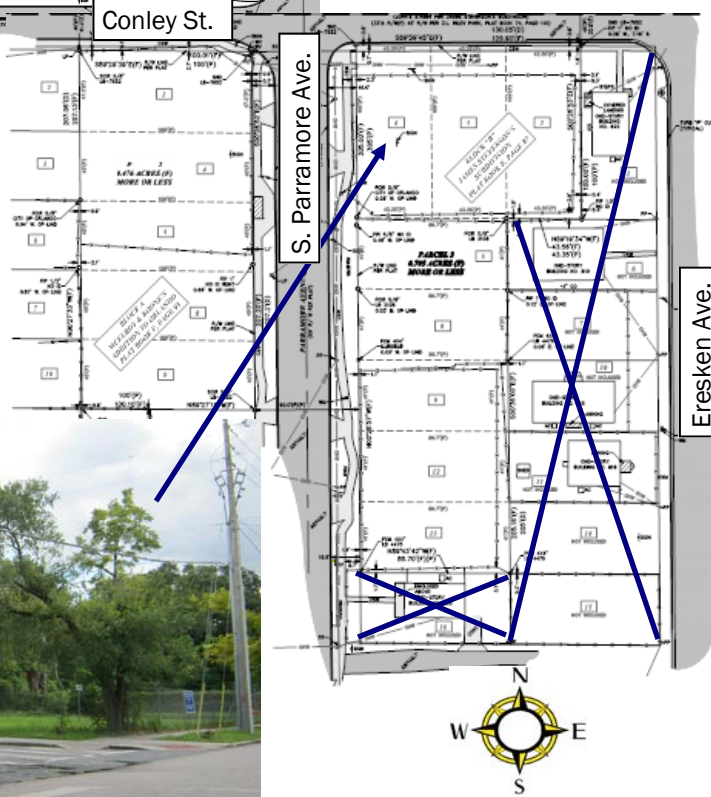
# SITE SURVEY & PHOTOS



Former Parramore Village walkway (from S. Parramore Ave.)



Ex-Parramore Village property from further south



SE corner of S. Parramore Ave. and Conley St. (future townhome sites)



# SITE PHOTOS



Existing fence/wall on ex Parramore Village property. Richard Allen Apts in the background.



Terminus of America St. from the west side of the former Parramore Village property.



Vacant property on north side of America St., next to Richard Allen Apts.



# SITE PHOTOS

View of Z.L. Riley Park main entrance on east side of S. Parramore Ave., across from former Parramore Village.



Conley St. looking west, with Carver Park homes on the left, and former Parramore Village to the right.

Rear access (ex-Weaver Ct.) to ex Parramore Village property, off Short Ave. Townhomes are proposed for this side of Short Ave.



# APPLICANT-PROVIDED INFORMATION

## Development Proposal

- The name for the Project is Parramore Oaks
- It will consist of 211 total units
- 178 units in the 3 and 4-story structures and 33 townhomes
- It will consist of 30 one bedroom/one bath units, 142 two bedroom/two bath units, and 39 three bedroom/two bath units
- 5% of the units will be set aside for use as permanent supportive housing units targeted to persons transitioning out of homelessness – 11 Units
- 10% of the units will be set aside for households with incomes at or below 40% of AMI (ELI) – 22 Units
- 80% of the units will be set aside for households with incomes at or below 60% of AMI, which includes the set asides for permanent supportive housing units and ELI units – 147 Units
- 20% of the units will not be income restricted (market rate) – 42 Units

## Development Phasing

- First phase - 120 units, comprising the 3-story apartment building at the intersection of Carter and Parramore (49 units), the 4-story building at the intersection of Parramore and Conley (52 units), and the 19 townhomes at the Southwest and Southeast corner of Parramore Avenue and Conley Street
- Second phase - 91 units, consisting of a 4-story buildings on the north side of Conley Street east of Short with 77 units and 14 townhomes on the west side of Short Avenue in two seven unit buildings

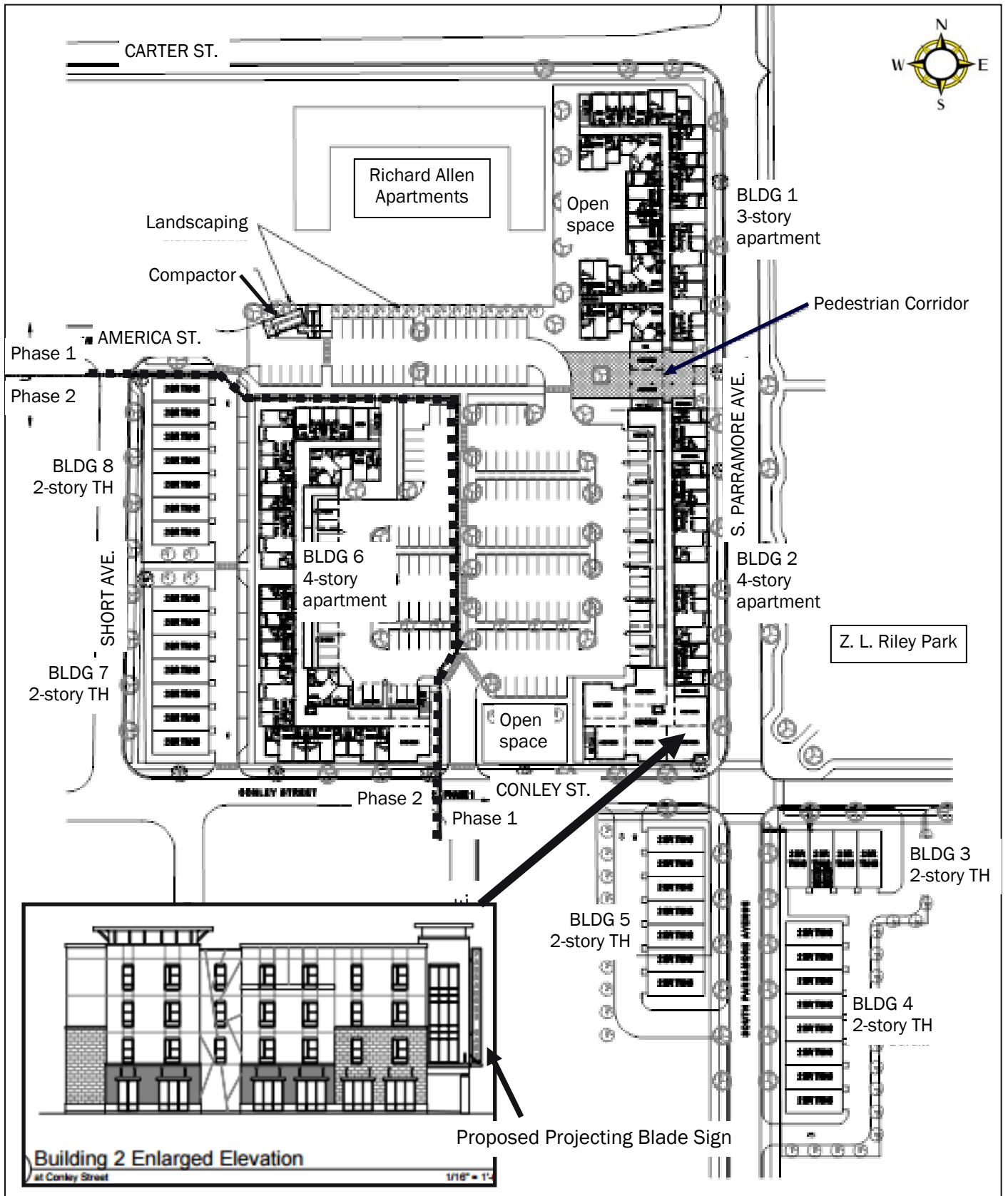
## Development Timing (Anticipated)

- Phase I Construction Start – July 2017
- Phase I Construction Completion – October 2018
- Phase II Start – July 2018
- Phase II Construction Completion – October 2019

### **Sustainable design in the Florida environment requires the use of some of these materials depending on the agreed upon final design of the building.**

- Concrete block and hollow core plank concrete construction
- Roof surfaces: Wood truss with 30 Year asphalt architectural shingles
- Exterior Walls: Stucco or Hardie board siding and trim in appropriate areas
- Slab: Slab on grade unless elevation changes or flood plain dictate the use of some stemwalls
- Limited use of architectural stone or brick finishes
- Hip or flat roofs to limit wind exposure
- PGT or equal windows for energy efficiency and design

# SITE PLAN





# PROPOSED BUILDING ELEVATION/PERSPECTIVE



3-story multi-family units on S. Parramore Ave., with Carter St. and SR 408 on the right.

Conceptual Elevation



## PROPOSED LANDSCAPE PLAN

Richard Allen Apartments

Z. L. Riley Park

Carver Park Home Sites



# ADDITIONAL ELEVATIONS



3 Building 1 Enlarged Elevation  
at Parramore Avenue

1/16" = 1'-0"



2 Building 2 Enlarged Elevation  
at Parramore Avenue



Proposed 3- & 4-story Multi-Family Units on the west side of S. Parramore Ave. & north side of Conley St.



Proposed 2-Story Townhomes on the east side of Short Ave. and east and west sides of S. Parramore south of Conley St.

# PROPOSED UNIT AND PARKING COUNTS

PHASE I					
Building #	Unit Type	Unit Name	Unit Total	Unit Type Percentages	
Building 1 3-story Multi-Family	1 Bed / 1 Bath	A	10		
	2 Bed / 2 Bath	B	33		
	3 Bed / 2 Bath	C	6		
Building 2 4-story Multi-Family	1 Bed / 1 Bath	A	6		
	2 Bed / 2 Bath	B	29		
	3 Bed / 2 Bath	C	17		
Building 3 2-story Townhouse	-	-	-		
	2 Bed / 2 Bath	B	4		
Building 4 2-story Townhouse	-	-	-		
	2 Bed / 2 Bath	B	8		
	-	-	-		
Building 5 2-story Townhouse	-	-	-		
	2 Bed / 2 Bath	B	7		
Phase I Total			Total A	16	13.33%
			Total B	81	67.50%
			Total C	23	19.17%
			Total All	120	100.00%
PHASE II					
Building 6 4-story Multi-Family	1 Bed / 1 Bath	A	15		
	2 Bed / 2 Bath	B	47		
	3 Bed / 2 Bath	C	15		
Building 7 2-story Townhouse	-	-	-		
	2 Bed / 2 Bath	B	7		
Building 8 2-story Townhouse	-	-	-		
	2 Bed / 2 Bath	B	7		
Phase II Total			Total A	15	16.48%
			Total B	61	67.03%
			Total C	15	16.48%
			Total All	91	100.00%
PHASE I & II					
			Total A	31	14.69%
			Total B	142	67.30%
			Total C	38	18.01%
			Total All	211	100.00%

## PARKING

PHASE	OFF-STREET	ON-STREET	COMBINED	RATIO
I*	139	12**	151	1.49
II*	71	0	71	0.92
TOTAL			222	1.25

\* PARKING SPACE CALCULATIONS ABOVE ARE FOR BUILDINGS 1, 2 & 6 ONLY. TOWNHOUSES IN PHASES I & II HAVE INTERNAL PARKING AND ARE NOT INCLUDED IN THE ABOVE CALCULATIONS

\*\* 12 ON-STREET SPACES ON PARRAMORE AVE (OUT OF 20) HAVE BEEN FACTORED INTO FINAL PARKING COUNT TO ACHIEVE 1.25 PARKING RATIO

## FINDINGS

Staff finds that the proposed Parramore Oaks Framework PD is consistent with the requirements for approval of Planned Development applications as contained in Section 65.290 of the Land Development Code (LDC):

1. The proposed PD is consistent with the City's Growth Management Plan
2. The proposed PD is consistent with the City's Land Development Code.
3. The proposed development meets the standards for development in the default R-3C/T/PH zoning district.
4. Existing public facilities and services are adequate to serve the proposed development.

### Staff Recommendation

Based on the information provided in the staff report and the findings noted above, staff recommends approval of the Parramore Oaks Framework PD, subject to the conditions in this staff report.

## CONDITIONS OF APPROVAL (REQUIRED)

### Growth Management

School Concurrency/Capacity - The project results in 26 single family units and 61 multifamily units, or 87 dwelling units being vested (from the previous Wells Landing PD and additional land being added to the new PD). A capacity agreement will need to be approved prior to the 2nd reading of the PD Ordinance. A concurrency agreement must be approved by OCPS and provided to the project planner prior to the recording of the plat. School impact fees may also be required for the overall 211-unit apartment and townhouse development.

### Land Development

1. Land Use and Zoning - Unless otherwise stated in this report or in the final adopted PD Ordinance, development of the property must be consistent with the development standards of the default R-3C/T/PH zoning designation.
2. Density - Overall density must not exceed 33.2 dwelling units per acre.
3. Impervious Surface - Impervious Surface coverage must not exceed 80%, per the default R-3C/T/PH zoning.
4. Signage - A separate sign master plan is required (done via Planning Official Determination) prior to any permits being issued. All signage must be permitted prior to fabrication and installation.
5. Purpose of the Framework PD - The framework PD will provide relief from zoning setback and building height requirements (based on the default R-3C/T/PH zoning), will provide a minimum required parking reduction for the multi-family portion and will also allow an affordable housing density bonus.
6. Landscaping Buffers - Bufferyard A (5-ft. buffer with 80% tree coverage and a 6-ft. tall wall OR a 7-ft. buffer with 80% tree coverage and shrubs and groundcover is required between the proposed townhomes off S. Parramore Ave. and Conley St. and adjacent single-family homes.
7. Building height for the multi-family buildings will not exceed 55 ft.
8. Phasing - The project will be constructed in two (2) phases, such that each phase can function independently of the previous or subsequent phases.
9. Parking/Reduction - Per the proposed mix of apartment types (# of bedrooms), 222 parking spaces are to be provided solely for the multi-family portion of the framework PD. The 1st phase of the multi-family use will be deficient 28 parking spaces (see Transportation Planning conditions further down relative to parking).
10. Specific Parcel Master Plan (SPMP) - An SPMP will be required for each phase of development prior to applying for building permits. The SPMP will require a public hearing before the Municipal Planning Board (MPB) unless it meets the requirements/allowances for the development to be handled administratively, per LDC Section 65.331.

### Urban Design

1. An Appearance Review is required prior to building permits being issued.
2. Downtown Appearance Review Board [ARB] Approval
  - a. This project is located in the Downtown Community Redevelopment Area (CRA), which requires Downtown Appearance Review Board (ARB) approval for new construction projects. Once funded, the initial phase of the Specific Parcel Master Plan (SPMP) for this project must receive an ARB Courtesy Review prior to the project proceeding to the Municipal Planning Board (MPB).
  - b. Prior to submittal of building permits for vertical construction, this project must receive a Major Certificate of



## CONDITIONS OF APPROVAL (CONTINUED)

Appearance Approval from the ARB.

### 2. Streetscape

#### A. Streetscape Design Guidelines

- i) All streetscape design and construction is required to comply with the design and construction requirements of the Downtown Orlando Streetscape Design Guidelines as they may be amended from time-to-time.
- ii) A city services easement must be provided by the applicant for any portion of the 15-foot streetscape zone outside of applicable rights-of-way.

#### B. S. Parramore Ave. Streetscape

- i) Width - The streetscape width on S. Parramore Ave., between Carter and Conley Sts., must be a minimum of 15 ft from the back-of-curb. The furniture zone must be a minimum 6 ft wide and must include 6 ft by 9 ft street tree wells and double acorn street lights. The pedestrian clear zone must be a minimum of 9 ft in width. South of Conley St to the S. Parramore Ave townhouse driveway cuts, the streetscape must be configured with a 7-ft wide grassed parkway strip and an 8-ft wide concrete sidewalk.
- ii) Treatments - The S. Parramore Ave streetscape between Carter and Conley Sts. must meet the requirements of Treatment 4 per the Streetscape Design Guidelines. The sidewalk cells must be 5-ft by 5-ft with a troweled edge and medium broom finish from the back of curb to back of sidewalk or face of building. The streetscape treatment south of Conley St shall meet the standards and requirements for Treatment 5 in the Streetscape Design Guidelines.
- iii) Corner Treatments - Lawrenceville brick, consistent with the Streetscape Design Guidelines standards, must be used in the corner treatments at Conley and Carter Sts.
- iv) On-Street Parking - Striped on-street parking spaces are required along the west side of S. Parramore Ave. Long runs of on-street parking spaces must be broken up with tree islands every four (4) spaces.

#### C. Conley Street and Short Avenue

- i) Width - The minimum streetscape width on Conley St and Short Ave must be 13-ft from the back-of-curb with a 7-foot wide grassed parkway strip, 6-foot wide concrete sidewalk and single acorn streetlights in the parkway strips.
- ii) Streetscape - The Conley St and Short Ave streetscapes must meet the standards of Treatment 5 in the Streetscape Design Guidelines.

#### D. General Streetscape Requirements:

- i) Street Trees – High rise live oaks trees must be planted as the primary street tree in the furniture zone or parkway strips on Parramore Ave, Conley St and Short Ave. Sylvester Palms may be used as accent street trees at project entrances.
- ii) Structural Soil – To minimize root damage to adjacent pavement areas, structural soil or a Planning Official-approved equivalent must be installed around all street trees between Carter and Conley Sts consistent with detail 3.4-O and -P of the Streetscape Design Guidelines.
- iii) Street Lights – Double acorn 40-watt LED streetlights, consistent with the Streetscape Design Guidelines and spaced based on OUC lighting requirements, must be used on S. Parramore Ave in the furniture zone or grassed parkway strips. Single acorn 40-watt LED streetlights should be utilized on Conley St and Short Ave in the grassed parkway strip.
- iv) Curbing - Existing driveway cuts not utilized for access to the subject site must be removed and the curbing replaced with vertical curb. Existing damaged curb must also be repaired or replaced with the construction of the streetscape.
- v) Corner Treatments - Corner treatments at street intersections must provide two-directional accessibility ramps at each corner perpendicular to the centerline of the adjacent roadway.
- vi) Valve and Junction Boxes - All at-grade junction, valve and control boxes in the streetscape zone must be traffic-bearing grade boxes and lids, painted per the Streetscape Design Guidelines. Valve and junction boxes in the grassed parkway strip must include a concrete collar.
- vii) Pedestrian Crossings - The pedestrian crossings at driveway cuts must be level and at the same grade as the sidewalk adjacent to the driveway. An alternative pavement treatment such a Lawrenceville brick or

## REQUIRED CONDITIONS (CONTINUED)

stamped and colored concrete that contrasts with the vehicle lanes must be used in order to clearly define the pedestrian zone. Reflective paint alone is not acceptable at internal and external pedestrian crossings, but may be used in conjunction with pavers or alternative paved surfaces to outline the pedestrian path for night time safety.

- viii) Building Entries - Building entries should face the street and be recessed, or the doors must open inward so that the entry doors do not conflict or open directly into the pedestrian clear zone.
- ix) Pedestrian Clear Zone - In no instance can vertical structures or obstructions be allowed in the required pedestrian clear zones or sidewalks.
- x) Streetscape Materials - Specialty paving and other materials not consistent with the Streetscape Design Guidelines are not permitted inside the right-of-way or city services easements of the streetscape zone.
- xi) ARB Final Review - Final streetscape plans must be submitted for ARB final review and approval prior to submittal of building permits.

### 3. Architecture

- A. Design Intent - Continued focus on the design and details at the base, middle and roofline of the buildings will continue to be critical through the ongoing design and development process. To that end, schematic architectural plans, elevations, axiometric renderings, materials and finishes must be submitted for ARB courtesy review prior to the MPB hearing for the initial phase of the development (after funding is secured). Final architectural plans, elevations, axiometric renderings, materials and finishes must again be submitted for ARB Final Review and approval prior to the submittal of building permits.
- B. Multi-Family Buildings
  - i) A minimum of three (3) different materials, not including paint colors, must be utilized on all outward facing facades.
  - ii) Ground floor units with patios must have a door that faces the adjacent street or alley. It is recommended but not required that ground floor units that face Parramore Ave., Conley St. and America St. also have a gate and sidewalk connection to the adjacent sidewalk.
  - iii) All HVAC equipment must be mounted on the roof of the buildings and the parapet must be the same height as the installed HVAC equipment in order to screen the units at eye level.
- C. Townhouses
  - i) The townhouse elevations must complement but not mimic the multi-family building elevations in order to provide color and architectural variety along the street frontage. Additionally, the parapet height must be varied in order to provide additional visual interest.
  - ii) A minimum of three (3) different materials, not including paint colors, must be utilized on all street-facing facades.
  - iii) All HVAC equipment must be mounted on the roof of the buildings and the parapet must be the same height as the installed HVAC equipment in order to screen the units at eye level.
  - iv) All garage doors must include glazing.
- D. General Architectural Requirements
  - i) Balconies, Canopies and Awnings on the buildings may project into the City Service Easements. Canopies or awnings that project into the right-of-way will require Transportation Engineering approval. Balconies and canopies that encroach into City Service Easements must be no less than 12-ft above finished grade to meet maintenance access requirements. Awnings must be a minimum of 9-ft above finished grade.
  - ii) Windows - The windows on all facades must be recessed 1 to 3 inches from the façade to provide additional design texture and shadow lines on the building façades. The addition of window sills or trim treatments is also recommended.
  - iii) Exterior Doors - Minimum 4-in. x 6-in. security view panels must be provided in all common exterior doors, including emergency exit doors, to provide visibility and security for pedestrians exiting building(s).
- E. Transparency
  - i) A minimum of 15% transparency must be provided on all floors facing the street. For consistency in the transparency calculation, it is assumed that all residential floors will have an 8-ft floor to ceiling height.
  - ii) All glass at the ground level must be clear. Minimum light transmittance must be 80%. High performance

## REQUIRED CONDITIONS (CONTINUED)

or low-e glass may be considered as an alternative with a minimum transmittance of 60%. Tinted, reflective, frosted or spandrel glass will not count towards meeting the transparency requirements.

- iii) No windows at the ground floor level can be dry-walled or have permanent partitions installed on the interior to block natural surveillance.

### 4. Tree Preservation/Mitigation

- A. Tree Surveys – A tree survey overlaid on the site plan must be provided in the SPMP submittal for each phase.
- B. Preservation - Several mature specimen trees exist on the site, especially around the project edges. The applicant must use their best efforts to preserve as many existing mature trees as is feasible. Each preserved tree will enhance the value and aesthetic vitality of the overall project.
- C. Tree Removal/Mitigation - A tree removal or encroachment permit is required prior to encroachment on or removal of any trees larger than 4 inches. As part of the tree removal or encroachment permit process, the Applicant must work with the City arborist to evaluate the viability of the existing on-site trees and determine if any mitigation will be required for the removal of any trees.

### 5. Lighting

- A. Photometric Plan -- A lighting plan compliant with the City's lighting regulations [LDC Chapter 63 Part 2M], including photometrics and all proposed exterior lighting fixtures, must be submitted for ARB review and approval prior to submittal of building permits.
- B. Parking Lot Lighting – Fixtures must be a maximum 20 ft tall.

### 6. Mechanical Equipment and Site Fixtures

- A. Venting & Exhaust for mechanical equipment and other utilities must be directed towards the rear of the buildings and integrated into the building design. All vents must be painted to match the surrounding façade color.
- B. Final Elevations - The location and configuration of all exterior venting and mechanical equipment must be depicted on all building elevations, and must be submitted for ARB Staff review prior to submittal of building permits.
- C. Transformer Areas outside the building envelope must be located on the interior of the project site and screened on three (3) sides with a hedge that will be 36-in. tall at the time of installation.
- D. Mechanical Equipment - All ground mounted and rooftop mechanical equipment must be screened from view and must meet the screening conditions of the Land Development Code. An interior screen wall or parapet for rooftop mechanical equipment may be required. Interior screen walls or parapets must be the same height as the installed mechanical equipment height.
- E. Backflow Preventers [BFP] must be located so as to not be directly visible from the adjacent right-of-way and should be screened with landscaping that matches the height of the BFP. BFPs must be clearly identified on the final utilities plan.
- F. Fencing on the site must be an open, CPTED-approved style, such as architectural mesh, welded wire or aluminum picket. Chain link fencing is prohibited.
- G. Clustered Mail Boxes must be located on the interior of the site and must not be located between the building and the adjacent right-of-way.
- H. Electric Meters must be located on the interior of the site and not visible from the adjacent right-of-way. Electric meter boxes must be painted to match the building colors.
- I. Telecommunications Equipment should be integrated into the building architecture.

### 7. Signage

- A. Sign Master Plan – A Sign Master Plan must be submitted and approved by Planning Official Determination prior to any signs being permitted and erected on the site. The design and dimensions of all project identification signs shall be included in the Sign Master Plan and a copy of the approved Sign Plan must be included in the ARB Final Review package for each phase/SPMP.
- B. Mid-Rise Projecting/Blade Signs - Mid-rise projecting/blade signs are not currently permitted in the City sign

## REQUIRED CONDITIONS (CONTINUED)

Code; therefore, the proposed mid-rise blade signs will need to be incorporated into the PD Ordinance, including the following conditions:

- i) A building site is allowed one [1] mid-rise projecting sign per each building face adjacent to a street, walkway or plaza.
  - ii) The area of each permitted mid-rise projecting sign must be counted as part of the low-rise signage allocation for the building site.
  - iii) Mid-rise projecting signs can only contain a single message and must be permitted to incorporate a logo.
  - iv) Mid-rise projecting sign faces must be parallel to themselves, with no opposing or angled sign faces.
  - v) Mid-rise projecting sign text and graphics must be push-through letters, illuminated channel letters with no visible racetrack or halo-lit dimensionally solid letters. The sign face background must be a solid color unless the Appearance Review Officer approves an alternative. Backlit cabinet sign faces are not permitted.
  - vi) Mid-rise projecting signs cannot be allowed to incorporate digital screens or components, but may include kinetic features in the sign design.
  - vii) A mid-rise projecting sign must be located so as to not conflict with the architectural elements or character of a building; decorative brackets may be utilized to attach the sign face to the structure but must comply with the following location and size requirements.
    - The sign face of a mid-rise projecting sign can be no wider than four (4) ft and must be no thicker than 18-inches. The sign face and/or mounting brackets can project no more than 5-ft from the building facade.
    - In no instance can a mid-rise projecting sign face be more than 40-ft tall.
    - The bottom of a mid-rise projecting sign can be no less than 17.5 ft. above finished grade.
    - The top of a mid-rise projecting sign can be no more than 60-ft above finished grade or no higher than the finished floor elevation of the top floor of a multi-story building, whichever is less.
  - viii) Mid-rise projecting signs shall be included in the Sign Master Plan.
8. **Model** - Prior to permitting, a physical 1 inch = 100'-ft. scale model of the project must be provided for the DDB/CRA model located in the Downtown Information Center.

### Transportation Planning

1. **Bicycle Parking** - Per City Code Sec 61.333, a minimum of 20 bike spaces (in secured bike racks) must be installed. The bike racks must conform to City standards and be located on an impervious surface so as not to interfere with pedestrian or other vehicular movements. The location of the racks may be distributed at more than one place on the site and must be shown on plans submitted at time of permitting.
2. **Parking**
  - a) All dimensions for parking stalls and aisles must conform with City Code standards or be approved by the City Transportation Official.
  - b) The total number of parking spaces for Phase I, Buildings #1 & #2, may be constructed as shown, provided that the 28 space deficit for City Code minimum parking requirements be held in reserve, should actual demands exceed the developer's estimation. If parking demand for Phase I becomes problematic as determined by City Code Enforcement and the Zoning Official, the property owner must construct up to 28 parking spaces to the west of the current phase line shown on the submitted plans.
  - c) The submitted parking lot plan does not appear to include any ADA accessible spaces. A facility consisting of between 101 and 150 spaces must include a minimum of 5 such stalls. These stalls must be located near building entrances and include a clear and accessible path to those entrances.
  - d) The required number of parking spaces for Phase 2 will be determined by City Staff and the development team at the time of submittal of plans to either the Permitting Services or Planning Divisions. Actual parking demand history for Phase I of this project will be used to determine the appropriate reduction, if any, that may be allowed from regular City Code requirements.

### Transportation Impact Fees

Any new construction, change in use, addition, or redevelopment of a site or structure shall be subject to a review for Transportation Impact Fees. An estimate of the Transportation Impact Fees for this project is not yet available pending



## REQUIRED CONDITIONS (CONTINUED)

ing information from the City Housing and Community Development Department. For questions and information regarding Transportation Impact Fee rates you may contact Nancy Ottini at (407) 246-3529 or at [nancy.jurus-ottini@cityoforlando.net](mailto:nancy.jurus-ottini@cityoforlando.net).

### Sewer

A sewer capacity analysis must be conducted to determine the impacts of the proposed development. See City of Orlando Sewer Service Policy and Growth Management Plan, Wastewater Element, for Level of Service (LOS) factors for use in the analysis. Contact the Wastewater Division to schedule a pre-application meeting to discuss analysis to be prepared for review by the Wastewater Division.

### Transportation Engineering

1. The Owner/Applicant must remove all abandoned, unused, or unapproved curbcuts / driveways and must restore all curbs, gutters, parkways and sidewalks to Orlando Engineering Standards Manual (ESM) requirements and standards.
2. Any existing sidewalk curb ramps adjacent to the site must be reconstructed to meet ADA standards.
3. A city services and sidewalk easement must be dedicated where an existing or newly constructed sidewalk along public right-of-way extends onto the development site.
4. Pedestrian crossing signs and markings must be installed across Parramore Ave at the Conley St intersection.
5. For Phase 1, on-street parking stalls must be delineated on the west side of Parramore Ave. from Carter St. to the southern property line of the project south of Conley St. These stalls will be created east of the existing curb line, will be subject to the dimensions and sightline restrictions in the ESM, 5<sup>th</sup> ed., and will be segmented by curb extensions at various points along the length of the project. Curb extensions will typically be required at intersections, driveways, and where drainage structures allow. The intent of the curb extensions is to define the parking stalls as a separate area from the travel lane and to “break-up” the stalls into pods of 4 to 6 vehicles. Landscaping /hardscaping will also be required in the “bulb outs” created by extending the curb to the east. Similar requirements for on-street parking along Short Ave. will be addressed at the time of review of Phase 2 of the project. No on-street parking is required for Conley St.
6. Where a bus stop is adjacent to on-street parking, a minimum 80-ft. long bus bay must be designated. The applicant must coordinate with Lynx for any additional bus stop requirements.
7. On-site parking lot and parking space dimensions must comply with Orlando LDC and ESM.
8. On-site sidewalks must be a minimum of 5-ft. wide when adjacent to drive aisles and 7-ft. wide when adjacent to parking stalls unless wheel stops are used.
9. For all rear access shared driveways, the distance between the face of the garage structure and the far side of the shared driveway must be no less than 24-ft. for the purpose of maneuverability.
10. Shared driveway widths:
  - a. One-way, non-emergency access = 12-ft. min; 16-ft. preferred
  - b. Two-way, non-emergency access = 16-ft. min; 20-ft. preferred
  - c. Emergency access route both one and two-way = 20-ft. min; 24-ft. preferred.
11. At all project entrances, clear sight distances for drivers and pedestrians must not be blocked by signs, buildings, building columns, landscaping, or other visual impediments. No structure, fence, wall, or other visual impediment can obstruct vision between 2- and 8-ft. in height above street level. The street corner / driveway visibility area must be shown and noted on construction plans and any future site plan submittals. The applicant must design the site plan as necessary to comply with the Florida Greenbook and the FDOT Design Standards Index. Sight lines must be provided on both site plans and landscape plans.
12. The final site plan must show the location and size of the on-site solid waste compactor(s) / dumpster(s) with concrete pads, and enclosures with doors. The solid waste container(s) must not be located adjacent to any single family houses or directly adjacent to the public street. Dumpsters must be located to provide a minimum 50 ft. of clear backup space and constructed per Orlando Engineering Standards Manual (ESM) requirements, OR documentation must be provided from the City's Solid Waste Division indicating curb pick-up or other approved arrangement.
13. A solid waste collection and/or circulation plan for the townhouse units must be provided.

## REQUIRED CONDITIONS (CONTINUED)

14. A residential loading zone must be provided on the apartment site located convenient to freight elevator or equivalent building access. A residential loading and unloading zone is not allowed within the public right-of-way.
15. For any construction work planned or required within a public right-of-way or City sidewalk easement adjacent to a public right-of-way (including but not limited to: irrigation, drainage, utility, cable, sidewalk, driveway, road construction/reconstruction or landscaping), the Owner/Applicant must submit the following:
  - a. Maintenance of traffic plans (M.O.T.) (For more information/detailed requirements contact the Office of Special Events & Permits at 407-246-3704)
  - b. Roadway plans including paving, grading, pavement markings and signage (Contact the Permitting Transportation Engineering Reviewer at 407-246-3079 for details)
  - c. A copy of all required County and State permits (If permits are pending attach a copy of the application)

## INFORMATIONAL COMMENTS

### Land Development

1. General Code Compliance. Development of the proposed project should be consistent with the conditions in this report. The MetroWest Homeowners Association board and all codes and ordinances of the City of Orlando, the State of Florida, and all other applicable regulatory agencies. All other applicable state or federal permits must be obtained before commencing development.
2. Minor Modifications. Minor modifications and design changes including but not limited to fences, accessory structures, signs, landscaping, interior alterations, and other minor changes, that are required beyond those previously reviewed by the Municipal Planning Board, may be approved by the Planning Official without further review by the Municipal Planning Board. Major changes will require additional review by the Municipal Planning Board.
3. Conformance with Planned Development Required. Construction and development must conform to approved site plans, elevations, and landscaping plans on file with the City Planning Division and all conditions contained in this report, or as modified by the Municipal Planning Board and City Council. When submitting plans to the Permitting Division for permitting, the applicant MUST attach to each submittal a copy of this staff report, and the excerpts of the Municipal Planning Board minutes **or** a copy of the approved framework PD Ordinance.
4. Final Site Plan. Approval of the Planned Development by City Council typically gives the applicant authority to submit an application for Specific Parcel Master Plan (SPMP) approval. All final details and any changes required to said final details must be included in any approval for a building permit by the Permitting Division.
5. Planned Development (PD) Expiration. Upon approval of the framework PD by City Council, and approval of subsequent SPMPs for each phase, a building permit shall be obtained for the work requiring the framework PD within five (5) years of the PD approval, or the PD will expire and the land will revert to the default R-3C/T/PH zoning. No extensions shall be granted to the duration of the PD. If the building permit for the work requiring the PD expires before a Certificate of Occupancy or Certificate of Completion is issued for the work requiring said PD, then the PD is no longer valid and the applicant must apply for a new PD if the applicant wishes to proceed with a development requiring said PD.

**Permitting** (detailed conditions will be provided when each SPMP is submitted)

# INFORMATIONAL COMMENTS

## Orlando Police Department

The Orlando Police Department has reviewed the plans for the Parramore Oaks PD, to be located along S. Parramore Ave. and to the west and southeast, utilizing CPTED (Crime Prevention Through Environmental Design) principles. CPTED emphasizes the proper design and effective use of a created environment to reduce crime and enhance the quality of life. There are four overlapping strategies in CPTED that apply to any development: Natural Surveillance, Natural Access Control, Territorial Reinforcement and Target Hardening.

1. Natural Surveillance: Design the site to keep intruders easily observable. This is promoted by features that maximize visibility of people, parking areas and building entrances; doors and windows that look out onto streets and parking areas; pedestrian-friendly sidewalks and street; porches or patios and adequate nighttime lighting.

Overall Project: [A lighting plan was not available at the time of this review.]

- A. Lighting plays a very important role in CPTED. It is crucial that lighting sends the right messages to the public about the safe and appropriate use of space at different times of the day and night.

All lighting for this project shall meet or exceed the guidelines in Orlando City Code, Title II, Chapter 63.

- In order to create a sense of safety, pedestrian-scale lighting should be used in all high-pedestrian traffic areas throughout the development to include building entrances, pedestrian promenades, parking facility entrances, common areas, courtyards, amenity deck, walkways and service areas.
  - Appropriate lighting should be included in all areas anticipated to be used afterdark.
  - Lighting should not be screened out by landscaping (especially pedestrian scale lighting) or building structures such as overhangs or awnings.
  - Uniformity of light is crucial to avoid 'dark' spots, especially in parking areas.
  - Any illumination shall not cause a glare or excessive brightness that adversely affects the vision of pedestrians or motor vehicle operators on public or private property.
  - Pedestrian walkways, service entrances and access routes open to public spaces should be lit so that a person with normal vision is able to identify a face from a distance of 30 feet during nighttime hours.
  - The use of full cut-off or shielded light fixtures can direct light where it is intended while reducing light trespass, glare, and waste.
- B. Landscaping is another crucial aspect of CPTED. Trees branches should be kept trimmed to no lower than 6 feet from the ground and shrubs should be kept trimmed to no higher than 30 inches. Avoid conflicts between landscaping and lighting, especially lighting adjacent to canopy trees. Landscaping should not create blind spots or hiding places and should not block/cover windows. Open green spaces should be observable from nearby structures.
  - C. Outdoor furniture is a good way to increase surveillance and encourage community interaction. Consider furniture designs that encourage stopping and resting but reduce opportunities for potential offenders (i.e. a ribbed design rather than solid and center rails or arm rests to discourage sleeping).
  - D. Bicycle parking should be observable from entrances, securely fastened and not hidden behind landscaping or shear walls.
  - E. All sides of the buildings should have windows to observe the walkways, parking areas and driving lanes.

Residential Units: Entry doors on all residential units should contain 180° viewers/peep holes.

Leasing Office:

- A. Since there is typically no natural surveillance around dumpsters or service areas, be sure these areas are well-lit and that lighting is well maintained.
- B. Where possible, offices, reception areas, lobbies, or host stands should have exterior windows and furniture oriented to improve surveillance of public areas.
- C. Advertisements and product displays should not be located in windows. If advertisements must be used, they should be small in size and located in an area that will not hinder clear views into and out of the space.
- D. Decals which display height measures should be posted inside any public entrance.

## INFORMATIONAL COMMENTS (CONTINUED)

- E. Public restrooms should be visible from the main customer areas and away from outside exits.
2. Natural Access Control: Design the site to decrease crime opportunity by denying access to crime targets and creating in offenders, a perception of risk. This can be accomplished by designing street, walkway, building and parking lot entrances to clearly indicate public routes and discourage access to private areas with structural and landscape elements.
- A. Public entrances should be clearly defined by walkways, signs, landscaping, and architectural elements. Landscaping used around building and parking facility entrances should create clear way-finding, be well lit and not block entrances or create ambush points.
- B. There should be no easy access to the roof of the buildings.
- C. Signs located throughout the parking facility should remind users to lock their vehicles and keep valuables out of sight.
- D. Way-finding located throughout the project should help guide users to authorized areas while discouraging potential offenders. This is especially important in the parking facility since there will be different entrances and authorized parking areas based on intended use of the property.
- E. Signage with hours of operation should be clearly visible at any public entrance.
- F. The use of traffic calming measures as well as surface and gateway treatments is encouraged to promote safe vehicle speeds, reduce collision frequency and increase the safety and the perception of safety for non-motorized users.
- G. Walkways should be a minimum 6' in width to enhance pedestrian flow. Multi-purpose paths should be a minimum of 10' to accommodate different modes of travel simultaneously.
- H. Bollards are a good option to consider in key locations to protect pedestrians, life-safety elements, critical utilities and control or direct traffic.
3. Territorial Reinforcement: Design can create or extend a sphere of influence, where users develop a sense of territorial control, while potential offenders are discouraged. This is promoted by incorporating features that define property lines and distinguish private spaces from public spaces such as; landscape plantings, pavement designs, gateway treatments and CPTED open design (see-through) fences.
- A. The property should be designed to encourage interaction between authorized users.
- B. Each dwelling unit or member's only sections should have an address that is clearly visible, with numbers a minimum of five (5) -inches high, made of non-reflective material.
- C. Fencing could be used in select areas to add security, delineate property lines, allow transparency for surveillance, be unobtrusive, and create a sense of community. CPTED open style fencing is a good option to consider. These fences may contain pedestrian access points that utilize mechanical access control. Another option is landscape buffers, which include hostile vegetation, to delineate public from private spaces. The fencing and landscape buffer may be used together to further define and control spaces.
- D. Maintenance is an important aspect of territorial reinforcement. A well-maintained area sends the message that people notice and care about what happens in an area. This in turn discourages vandalism and other crimes.
4. Target Hardening: This can be accomplished by features that prohibit entry or access such as window locks, dead bolts for doors and interior door hinges.

### Overall Project:

- A. Door locks should be located a minimum of 40 inches from adjacent windows.
- B. Air conditioner units should be caged and the cages should be securely locked.
- C. An access control system should be considered so only authorized personnel have access to restricted or non-public areas. This can be incorporated into the parking facility and residential amenities areas as well.
- D. Common area doors or gates should have locks that automatically lock when the doors close.



## INFORMATIONAL COMMENTS (CONTINUED)

- E. If alarm or security systems are installed, each dwelling unit should have a separate system that can be regularly tested and maintained by the occupants.
- F. A security camera system capable of recording and retrieving an image to assist in offender identification and apprehension should be used throughout this project. Security cameras should be mounted at an optimal height to capture offender identification (“aiming” down from steep angles often results in images of the offender’s hat). CCTVs should also be placed in several locations throughout the parking facility, property access points and common areas; especially areas with sheer walls, limited or no natural surveillance.
- G. Large glass doors and windows should be made of impact resistant glass or a security film (such as Lexan™). If installing security film, ensure that the light transmittance of the security film is greater than or equal to the light transmittance of the window's glass.

### Residential Units:

- A. Exterior and non-public doors should contain 180° viewers/peep holes, interior hinges, single cylinder dead bolt locks with a minimum one-inch throw, metal frames with 3” screws in the strike plates, and be made of solid core material. This includes facility and storage room doors.
  - B. All windows that open should have locks.
  - C. Sliding glass doors should have one permanent door on the outside and the inside moving door should have a docking device and a pin or secondary lock.
  - D. An access control system should be considered so only residents have access to their building, floor, parking, common amenities, and pedestrian access points. Common area doors or gates (leasing office, fitness center, pool, etc.) should have locks that automatically lock when the doors close.
  - E. If alarm or security systems are installed, each dwelling unit should have a separate system that can be regularly tested and maintained by the occupants.
5. Construction Site Crime Prevention: Due to the continued trend of theft of building materials and equipment from construction sites, Orlando Police Department’s Crime Prevention Unit strongly recommends that the developer institute the following crime prevention/security measures at this project site:
- A. Post signs at the site that theft from the site or trespassing on a construction site is a felony under Florida Law and that the developer will prosecute.
  - B. To improve visibility of potential offenders by OPD patrol officers, perimeter lighting should be installed at a minimum of 150 foot intervals and at a height not less than fifteen (15’) from the ground. The light source used should have a minimum light output of 2,000 lumens, shall be protected by a vandal resistant cover, and shall be lighted during the hours of darkness.
  - C. In addition to lighting, one of the following physical security measures should be installed: a. Fencing, not less than six (6’) feet in height, which is designed to preclude human intrusion, should be installed along the perimeter boundaries of the site and should be secured with chain and fire department padlocks for emergency vehicle access; post in a clear area, an emergency contact person and phones numbers for after hours, in case of an emergency; or b. A uniformed security guard should be hired to continually patrol the construction site during the hours when construction work has ceased.
  - D. Valuable construction materials and tools should be protected in a secondary fenced, locked cage.
  - E. Post in a clean, open area, the name and numbers of an emergency contact person for OPD in case of a night-time emergency. If you have any questions, please call the Crime Prevention Unit Officer Richard Patterson, at 407.246.2096.
6. Innovative Response to Improve Safety (I.R.I.S.) Camera System: OPD recommends that the developer contact OPD in an effort to coordinate camera system technology. OPD has the ability to monitor specified external camera systems from private facilities at the will of the developer or building owner. OPD monitoring will be in addition to self- monitoring and will not replace building camera monitoring. OPD will not have the ability to interfere with or manipulate building camera systems, only view. The additional monitoring is beneficial to both the developer/owner and OPD as a crime prevention and overall public safety solution. IRIS questions should be

## INFORMATIONAL COMMENTS (CONTINUED)

directed to the Orlando Police Department, Sgt. Andy Brennan, at [andrew.brennan@cityoforlando.net](mailto:andrew.brennan@cityoforlando.net).

7. Crime Free Multi-Housing Crime Free Multi-Housing (CFMH) is an internationally recognized and certified crime fighting program designed to help tenants, owners and managers of rental property keep drugs and other illegal activity off their property. CFMH is designed to be easy, yet very effective in reducing the incident of crime in rental property. Orlando Police Department is one of a small handful of law enforcement agencies in Florida that have established this program. The program includes a Crime Free Lease Addendum which has shown to be the backbone of the Crime Free Multi-Housing Program. The addendum to the lease agreement lists specific criminal acts that, if committed on the property, will result in the immediate termination of the resident's lease. CFMH utilizes a unique three-part approach, which ensures the crime prevention goal, while maintaining a very tenant-friendly approach.

**Phase One** involves an eight-hour seminar presented by the police and fire departments. As soon as the management team is identified, please contact Officer Derwin Bradley to register the management team for the next scheduled seminar.

**Phase Two** certifies that the rental property has met the security requirements (based on CPTED strategies) for the tenant's safety. These requirements include:

- Dead-bolt locks on all unit entry doors with a minimum 2-inch throw;
- Eyeviews (peep holes) with 180 degree view;
- Strike plates with 3-inch screws; and
- Anti-slide devices on sliding doors and windows.

Implementation of the recommended CPTED strategies will prepare this community for inclusion in CFMH.

**Phase Three** concludes the program with a tenant crime prevention meeting. The sooner these meetings begin the better for this new community and the new residents. Contact Officer Bradley when the first group of tenants moves into their apartments. Officer Bradley will work with management to conduct the tenant programs. OPD strongly encourages all multi-family communities to become registered participants in the CFMH program. For a complete list of additional program requirements and benefits, please contact the OPD CFMH Coordinator, Officer Derwin Bradley at 407.246.3927 or [derwin.bradley@cityoforlando.net](mailto:derwin.bradley@cityoforlando.net) or the Crime Prevention Unit Officer Richard Patterson, at 407.246.2096.

## CONTACT INFORMATION

### **Growth Management**

Contact Mary-Stewart Droege at mary-stewart.droege@cityoforlando.net or at 407.246.3276.

### **Land Development (Project Planner)**

Contact Jim Burnett, Planner III, at (407) 246-3609 or at james.burnett@cityoforlando.net.

### **Urban Design**

Contact Doug Metzger at (407) 246-3414 or at doug.metzger@cityoforlando.net.

### **Transportation Planning**

Contact John Rhoades at (407) 246-2293 or at john.rhoades@cityoforlando.net.

### **Transportation Engineering**

Contact Jeremy Crowe at 407-246-3262 or at Jeremy.crowe@cityoforlando.net.

### **Orlando Police Department**

Contact Audra Nordaby at 407.246.2454 or at Audra.Nordaby@cityoforlando.net.

### **Permitting**

Contact Keith Grayson at keith.grayson@cityoforlando.net or at (407) 246.3234.

### **Stormwater**

Contact David Breitrick at david.breitrick@cityoforlando.net or at (407) 246-2754, or Vince Genco at vince.genco@cityoforlando.net or at 407-246-3722.

## REVIEW/APPROVAL PROCESS - NEXT STEPS

1. Minutes from the October 18, 2016 MPB meeting are scheduled for review and approval by City Council on Monday, November 14, 2016.
2. In this instance only, the City Attorney's office began preparation of the PD ordinance following the TRC meeting. The PD is tentatively scheduled for 1st reading at City Council on November 14, 2016, with the 2nd reading (a public hearing) before the City Council on December 5, 2016.