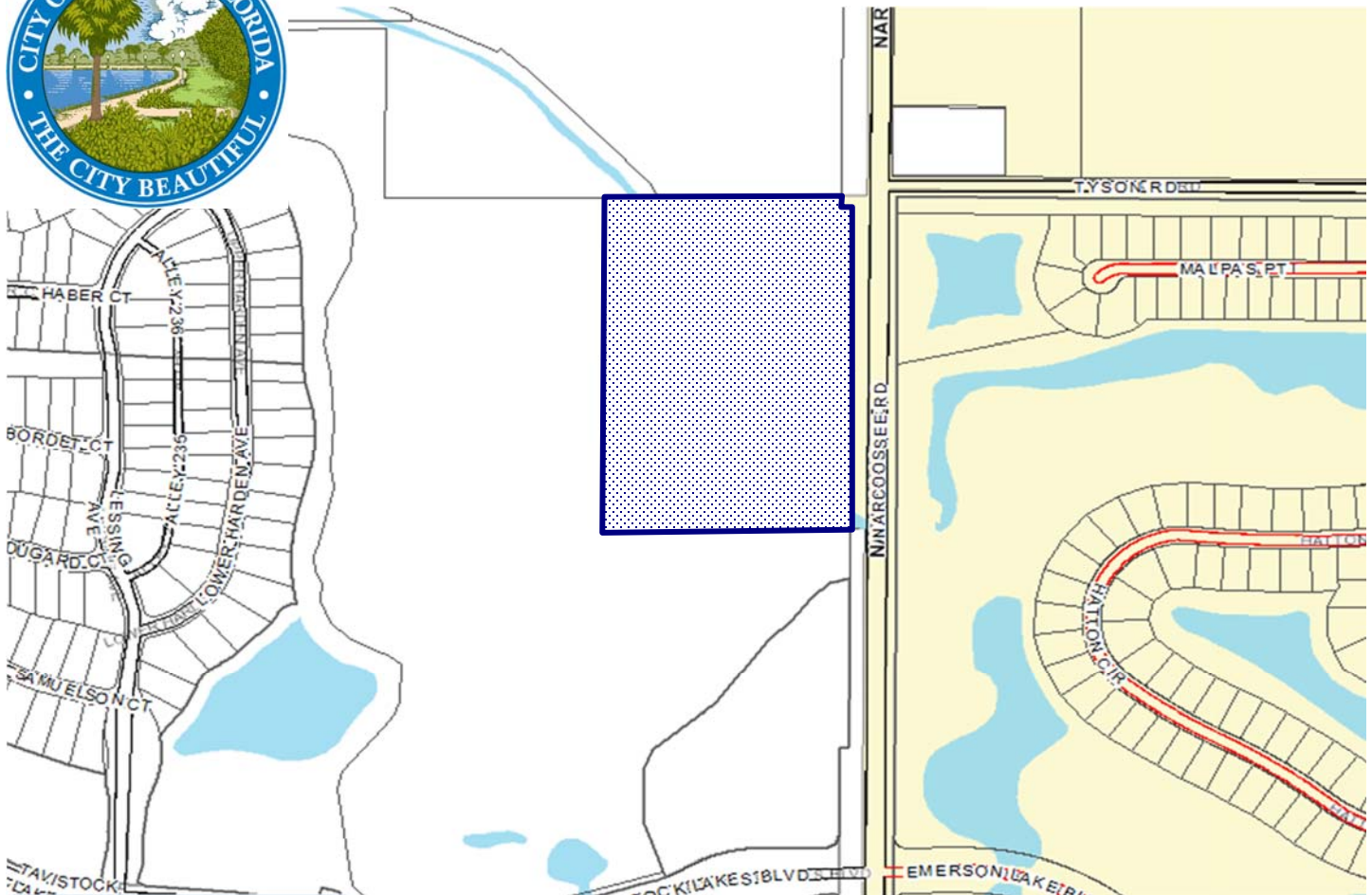




SHOPPES AT TYSON PLACE



Location Map

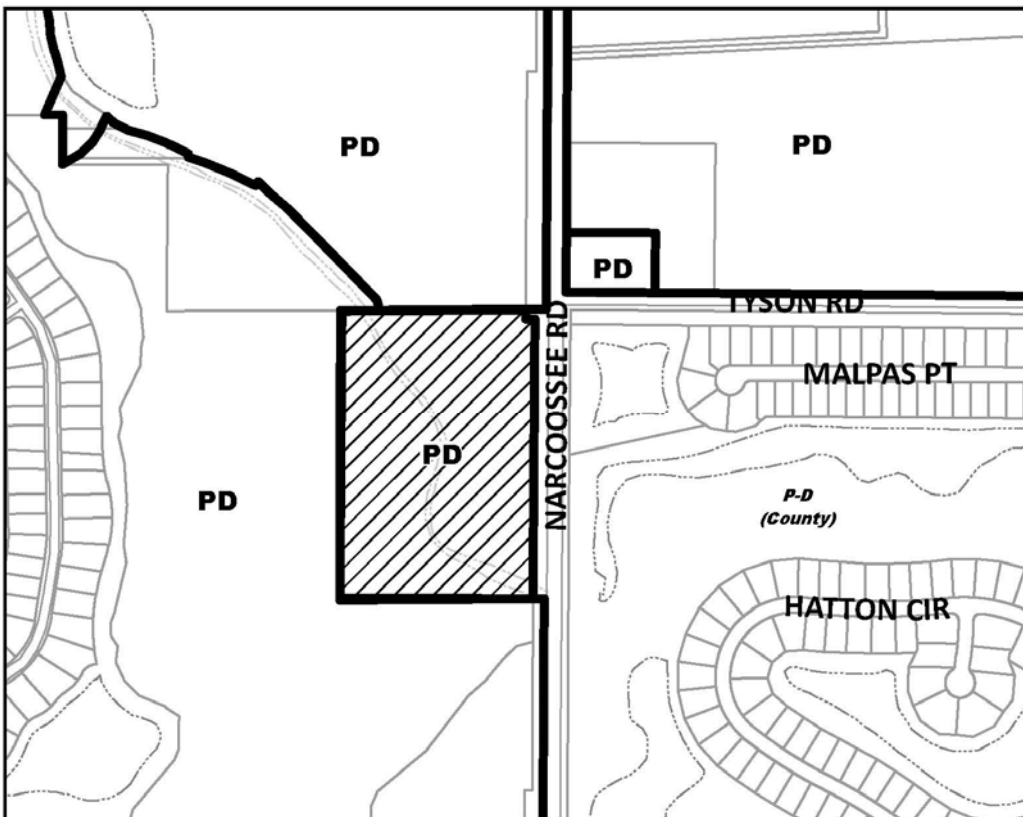


Subject Site

SUMMARY

<p>Owner Edgar Yates Colleen Yates, Clifton Ackerman & Joy Ackerman</p> <p>Applicant Javier Omana CPH, Inc.</p> <p>Project Planner Colandra Jones, AICP</p>	<p>Property Location: The subject property is located west of Narcoossee Road, north of Tavistock Lakes Boulevard, and south of Tyson Road (±14.63 acres, District 1).</p> <p>Applicant's Request: Specific Parcel Master Plan (SPMP) approval for 60,000 square feet of development comprised of retail, restaurants, bank and grocery store on 7 parcels within the Blackfin Shoppes PD.</p> <p>Staff's Recommendation: Approval subject to the conditions in the staff report.</p>	
<p>Updated: July 18, 2016</p>		

ZONING MAP



MASTER PLAN ANALYSIS

Project Description

The subject site is generally located west of Narcoossee Road, north of Tavistock Lakes Boulevard, and south of Tyson Road, and is approximately 14.63 acres. The site is currently designated Urban Village on the Official Future Land Use Map and is zoned PD and is designated as “Village Center” on the Blackfin Shoppes PD Development Plan. This proposed SPMP depicts a development with a total of a 60,000 square feet of retail, restaurants, a bank and a grocery store. The proposed development program is depicted in the chart below.

Proposed Development Program			
Parcel 1	0.81 acres	Retail	6,000 square feet
Parcel 2	2.31 acres	Grocery Store	24,000 square feet
Parcel 3	0.75 acres	Retail	5,000 square feet
Parcel 4	0.89 acres	Bank/Retail	6,000 square feet
Parcel 5	0.92 acres	Fast Food Restaurant	5,000 square feet
Parcel 6	1.12 acres	Restaurants/Retail	6,000 square feet
Parcel 7	1.15 acres	Retail	8,000 square feet
Total	7.95 acres		60,000 square feet

Previous Actions:

- October 2015—The Municipal Planning Board approved the annexation (ANX2015-00017), GMP amendment to assign the Urban Village and Conservation future land use designations (GMP2015-00033), GMP amendment to add property to Southeast Orlando Sector Plan (GMP2015-00034), and PD zoning for a 37,000 square foot mixed use development (ZON2015-00030).

Upcoming Cases:

- July 2016— The applicant has submitted an application to the Municipal Planning Board for a GMP amendment to change the future land use designation for 1.52 acres from Conservation to Urban Village (GMP2015-00054). Also a PD amendment to increase the development program from 37,000 square feet to 75,000 square feet. (Case #ZON2015-00057).

Project Context

As shown in the table below, the surrounding uses include Lake Nona High School to the north, Conservation to the west, the Lake Nona Gateway office building and Conservation to the south, and Eagle Creek single family residential neighborhood and golf course to the east located in the County. The proposed development would be compatible with the surrounding uses.

Table 1—Project Context			
	Future Land Use	Zoning	Surrounding Use
North	Urban Village	PD	Lake Nona High School
East	PD (Orange County)	PD (Orange County)	Single Family Residential & Golf Course
South	Urban Village & Conservation	PD	Lake Nona Gateway Office & Wetlands
West	Conservation	PD	Primary Conservation Network

Conformance with the GMP

The proposed SPMP is consistent with the GMP Policies related to the Southeast Orlando Sector Plan, including Future Land Use Element Policy 2.4.4 (Urban Village policy), Subarea Policies S.35.3 and S.35.4, along with Goal 4 and its associated goals, objectives and policies.

Conformance with the LDC—Chapter 68 Southeast Orlando Sector Plan

In order to be consistent with the Southeast Orlando Sector Plan, applications must be reviewed for compliance with LDC Chapter 68, which lays out the detailed development guidelines and standards for the Southeast Plan area. The proposed development is categorized as “Village Center” in the Education Village PD. According to LDC Section 68.200 (a) (2), Village Center/Urban Transit Center (VC/UTC District) is described as such: “Village Center districts shall be developed as important destinations for each Residential Neighborhood, providing a variety of shops, services, restaurants, and civic facilities that serve the needs of the surrounding neighborhoods.”

Development Standards

According to LDC Figure 68-A, development in the Village Center designation with non-residential development less than 0.4 FAR, the standards of the Conventional LDC are allowed. Standards shall be consistent with the City's AC-1 zoning district, with the exception of the FAR standard.

Intensity

According to Future Land Use Policy 4.1.9, there is no minimum intensity and the maximum intensity is 0.4 FAR (or no maximum if traditional design standards were used). The proposal has an FAR of 0.17.

Building Height

In the Southeast Sector Plan, height is limited by the number of stories, not the overall height to provide variety to the skyline in the mixed use centers. Commercial buildings shall have no more than 25 foot floor to floor heights. The Village Center designation allows for 1 to 3 stories. The proposed plan depicts one story buildings. The height of the retail store exceed the 25 foot floor to floor height, but is consistent with a two story building which is an allowed height limit.

Impervious Surface Ratio (ISR)

The maximum ISR permitted is 0.85 according to the Village Center standards. The development proposes an ISR of 0.85 which meets the ISR standards.

Table 2—Development Standards

Lot	Acreage	Use	Sq. Ft./ Dwelling Units	FAR (floor area ratio)		Building Height		ISR (impervious surface ratio)	
				Minimum / Maximum	Proposed	Minimum / Maximum	Proposed	Maximum	Proposed
1	0.81	Retail	6,000 sq. ft.	0 to 0.4 FAR	0.17 FAR	1 to 3 story	1 story	0.85	0.85
2	2.31	Grocery Store	24,000 sq. ft.	0 to 0.4 FAR	0.24 FAR	1 to 3 story	1 story		
3	0.75	Retail	5,000 sq. ft.	0 to 0.4 FAR	0.15 FAR	1 to 3 story	1 story		
4	0.89	Bank/Retail	6,000 sq. ft.	0 to 0.4 FAR	0.15 FAR	1 to 3 story	1 story		
5	0.92	Fast Food Restaurant	5,000 sq. ft.	0 to 0.4 FAR	0.12 FAR	1 to 3 story	1 story		
6	1.12	Restaurants/ Retail	6,000 sq. ft.	0 to 0.4 FAR	0.12 FAR	1 to 3 story	1 story		
7	1.15	Retail	8,000 sq. ft.	0 to 0.4 FAR	0.16 FAR	1 to 3 story	1 story		
Total	7.95		60,000 sq. ft.		0.21 FAR				

Setbacks

The required setbacks for the Village Center (AC-1 standards) are 0 feet for the front yard and street side yard, 0 or 3 feet for the side yard, and 20 feet for the rear yard setback. The rear setback and south side setback in this case would be the required 50 foot wetland setback. The proposal does not meet the required wetland setback for the rear and south side. The applicant is requesting an alternative standard for the wetland setback of 25 feet. Street frontage is required for each lot delineated in the master plan. As depicted in the proposed SPMP, Lot 3 does not have street frontage. The applicant shall revise the plan to adjust lot boundaries to allow for adequate street frontage for Lot 3.

Table 3—Setback Requirements

Use or Phase	Yard	Building Setbacks	
		Minimum	Proposed
Shopping Center	Front– Narcoossee Road	0 ft.	72 ft.
	Side–south property line	50 ft. Wetland Setback	25 ft.
	Street Side–north property line	0 ft.	79 ft.
	Rear– west property line	50 ft. Wetland Setback	25 ft.

Signage

The applicant did not submit a master sign plan for this project. All building signage will comply with Chapter 64 of the LDC. A Master Sign Plan must be submitted for review and approval prior to issuance of sign permits.

Parking

The applicant is proposing 257 parking spaces on the entire site except for Lots 4 and 6. The parking for these lots will be determined at the time of Appearance Review for Final Site Plan approval. Table 5 below provides the minimum and maximum parking ratios for the various individual uses on the property. To determine the maximum, the entire site is calculated as a shopping center (49,000 sq. ft. of shopping center at a maximum ratio of 4/1000 sq. ft. and 11,000 sq. ft. of restaurant at a maximum ratio of 20/1000 sq. ft.), the total maximum spaces permitted is 416 parking spaces. The proposed number of spaces of 257 is within the maximum permitted parking spaces. Although the entire site meets parking ratios, if the site is developed in phases, certain lots will exceed the maximum. Each phase must meet the minimum and maximum parking ratio, meaning parking must be reduced in Lots 1, 2 and 3.

Table 5—Parking Requirements Per Parcel

Parcel /Use	Sq. Ft./ Dwelling Units	Minimum Ratio	Minimum Spaces Required	Maximum Ratio	Maximum Spaces Permitted	Parking Spaces Proposed
1. Retail	6,000 sq. ft.	2.5/1000 sq. ft.	15	4/1000 sq. ft.	24	32
2. Grocery Store	24,000 sq. ft.	2.5/1000 sq. ft.	60	4/1000 sq. ft.	96	135
3. Retail	5,000 sq. ft.	2.5/1000 sq. ft.	12	4/1000 sq. ft.	20	21
5. Fast Food Restaurant	5,000 sq. ft.	5/1000 sq. ft.	25	20/1000 sq. ft.	100	45
7. Retail	8,000 sq. ft.	2.5/1000 sq. ft.	20	4/1000 sq. ft.	32	24
Total Provided	48,000 sq. ft.		132		272	257

Landscaping & Streetscape

The applicant submitted a conceptual landscape plan with this application that is found on page 8 of this report. The final landscape plan will be developed in accordance with Chapter 60 of the LDC. A final landscape plan, including plant list, will be submitted for Appearance Review prior to building permits. Street trees are required on Narcoossee Road.

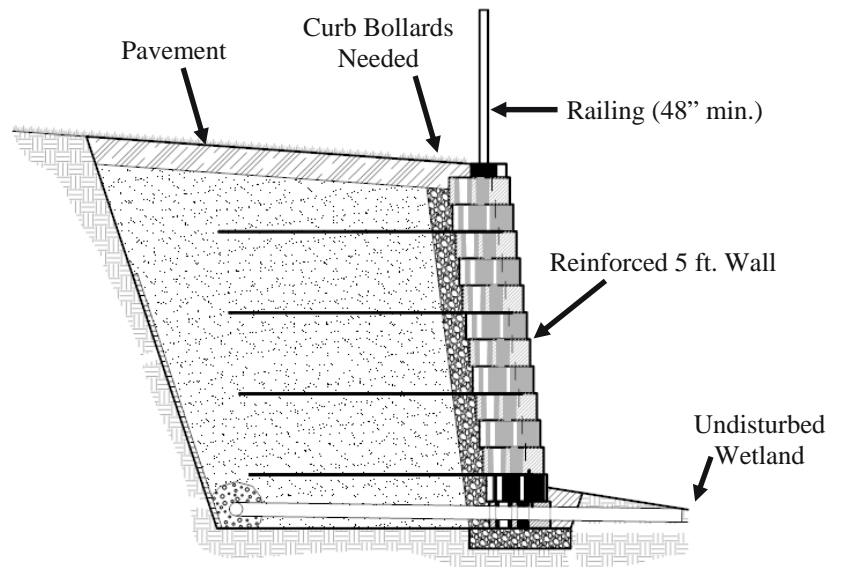
The applicant is also requesting an alternative standard to the required landscape area for vehicular use areas. According to LDC Section 61.312, the landscape area must be 7.5 feet adjacent to rights of way and property lines. The applicant is requesting a reprieve of this requirement only for the area adjacent to the wetland retaining wall. The property owner will provide additional interior landscaping to compensate for the lack of perimeter landscaping along the retaining wall. In addition to the retaining wall, the wetland area beyond the wall will be undisturbed and will provide landscape with the natural vegetation.

Wetlands

According to LDC Section 68.505, it requires a minimum 50 foot upland buffer for protected wetlands. The applicant is requesting an alternative standard from this requirement. The applicant is also requesting an alternative standard of this requirement. According to the site plan, it depicts a 25 foot setback from rear building of grocery store to wetland. One way the applicant is proposing to mitigate this standard is proposing a wall with a railing between the wetland boundary and the developable area. This wall section is depicted on the following page.

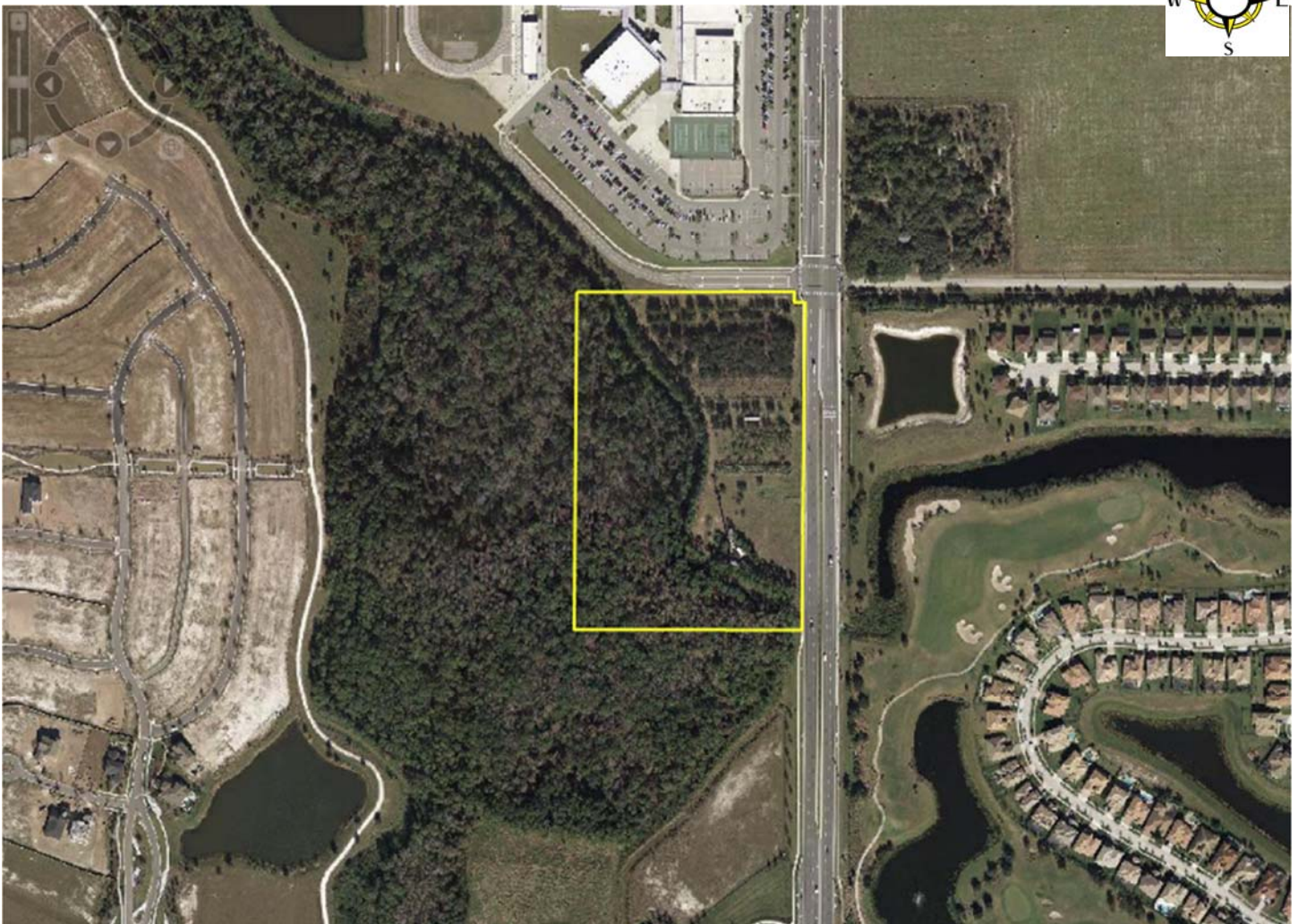
The site plan depicts wetlands that are created to help with the mitigation of the wetland impact. All created area total 0.28 acres. Vegetation to be planted in these created area will include cypress, loblolly bay, red maple, button bush, and soft rush. Canopy trees will be planted on 10-foot centers, sub-canopy will be planted on 5-foot centers and groundcover will be planted on 3-foot centers. The canopy species will be in three-gallon containers with a minimum 4 foot overall height.

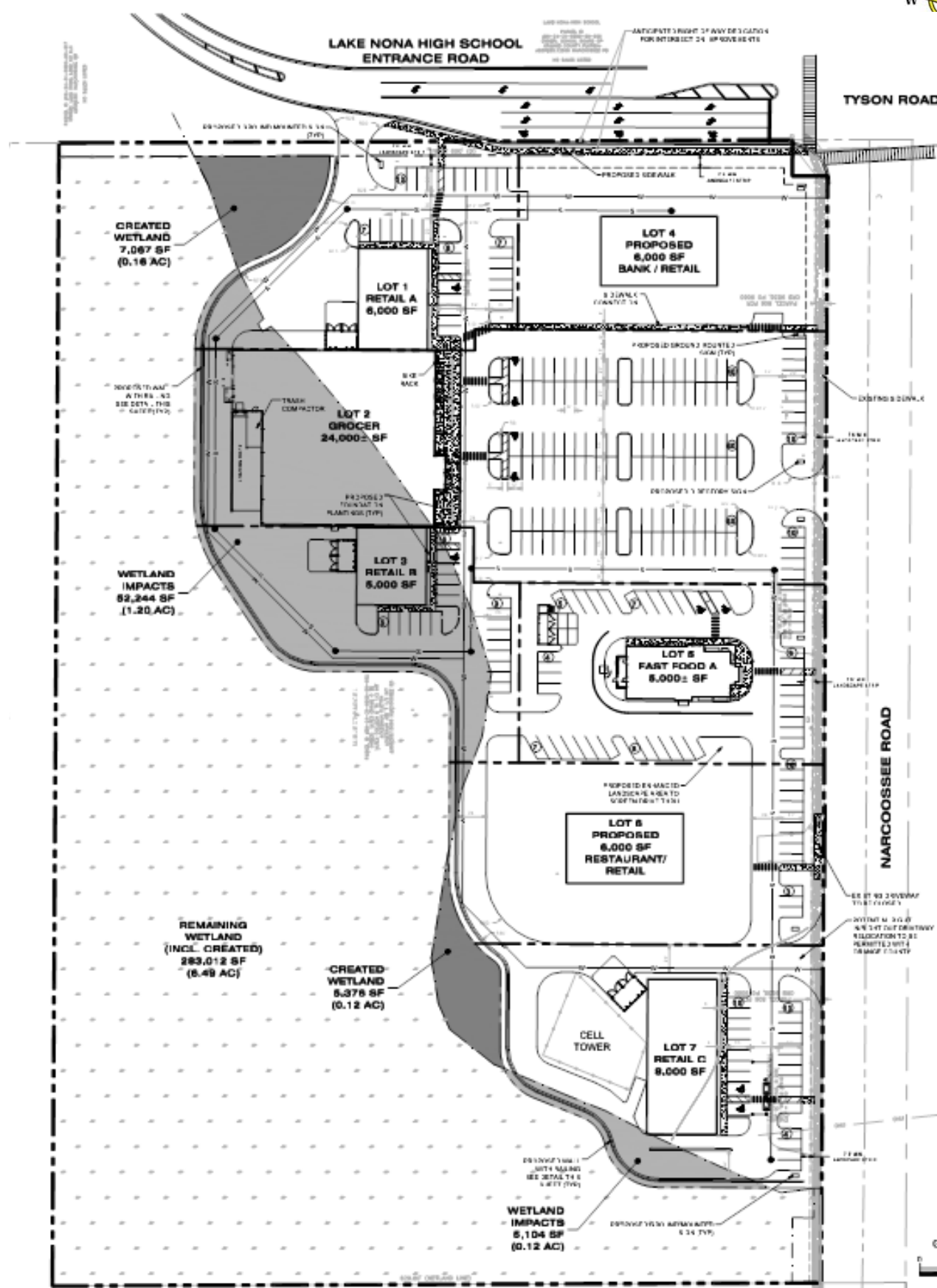
PROPOSED WALL BETWEEN WETLAND AND DEVELOPMENT



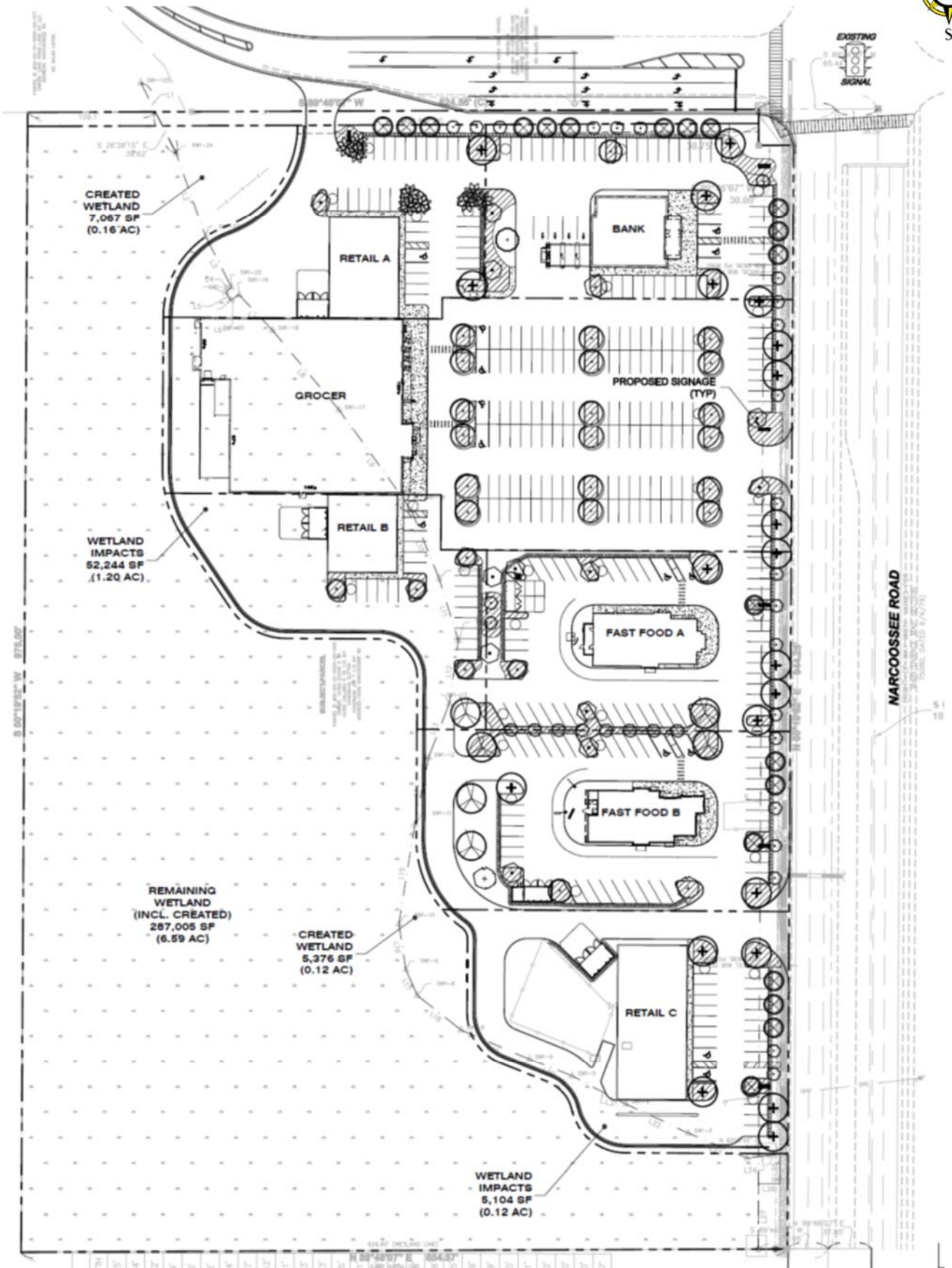
TYPICAL REINFORCED WALL SECTION

AERIAL PHOTO



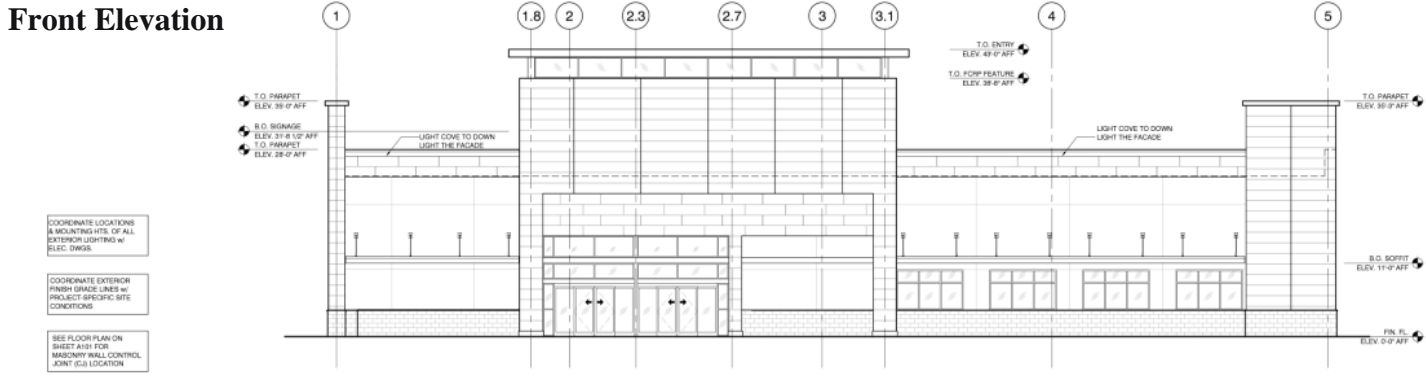


CONCEPTUAL LANDSCAPE PLAN

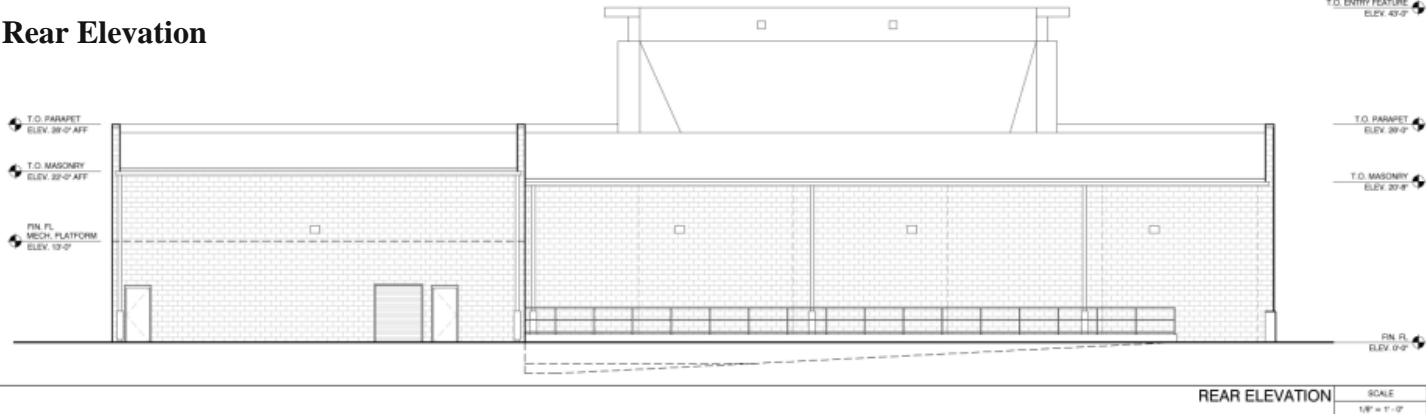


ARCHITECTURAL ELEVATIONS—LOT 2: GROCERY

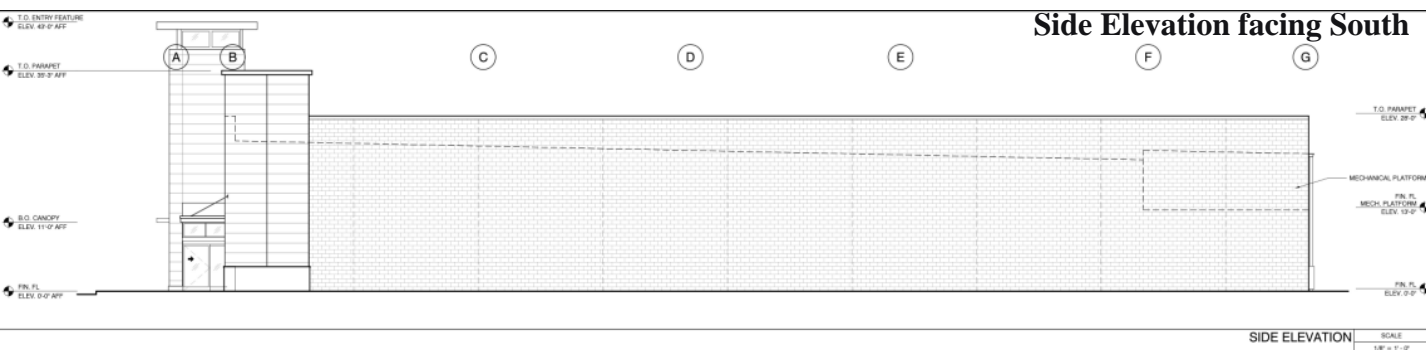
Front Elevation



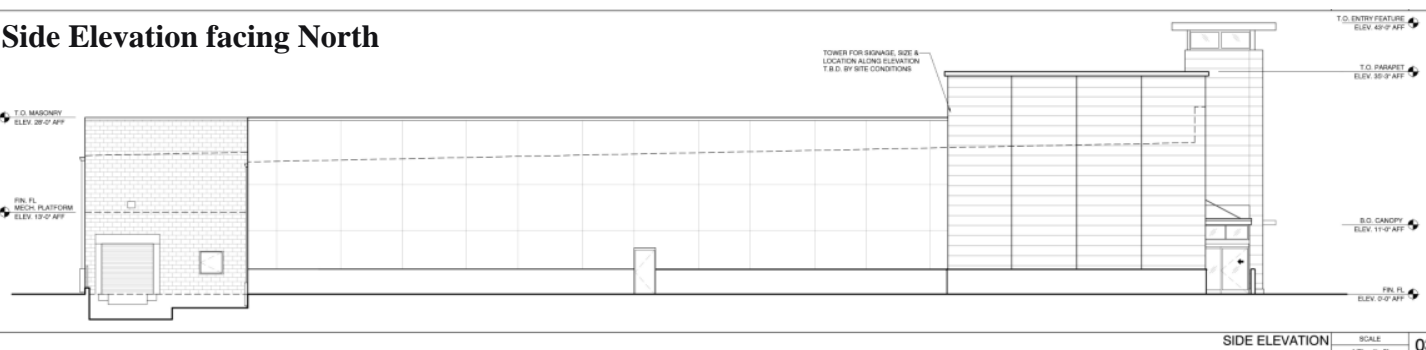
Rear Elevation



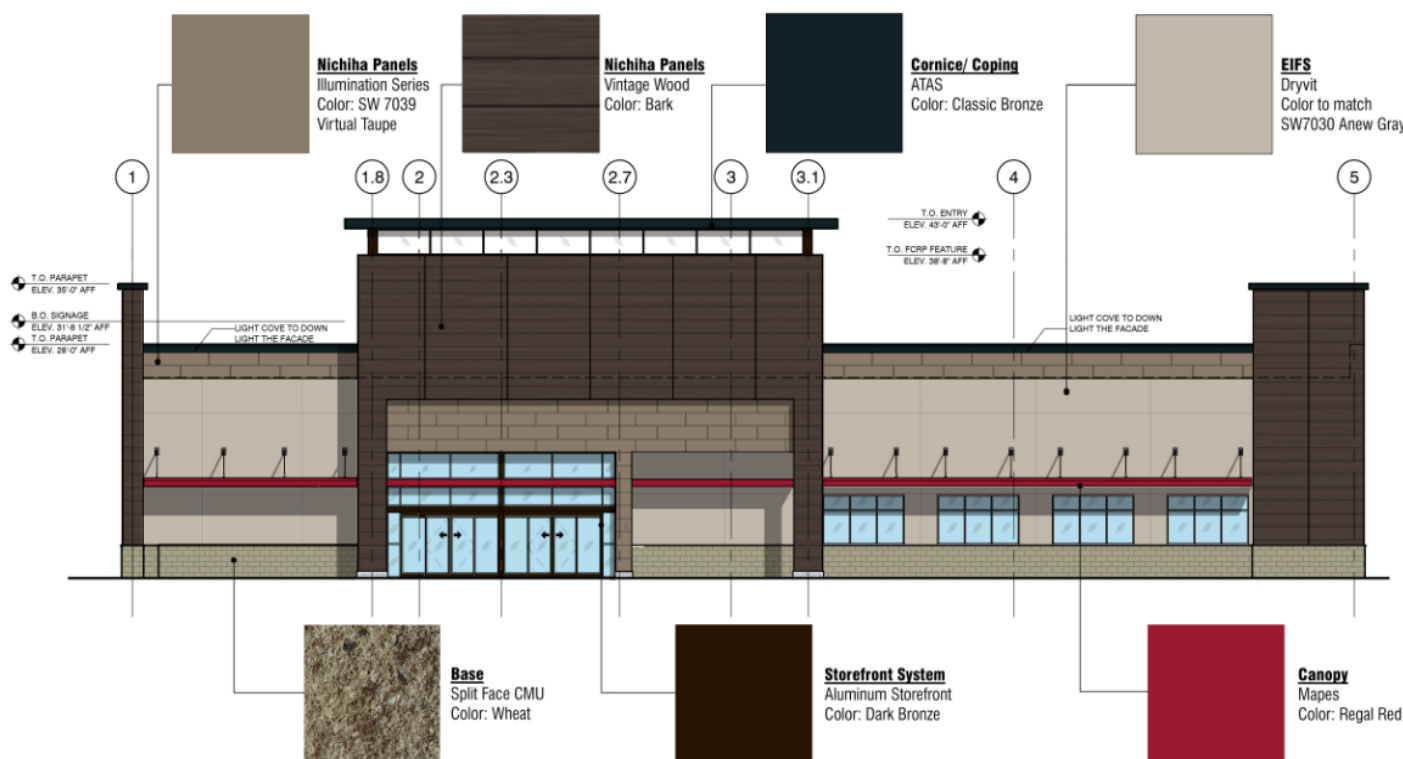
Side Elevation facing South



Side Elevation facing North



ARCHITECTURAL MATERIALS —LOT 2 GROCERY



FINDINGS

Subject to the conditions contained herein, the proposal is consistent with the requirements for approval of the proposed Specific Parcel Master Plan application contained in Section 65.336 of the Land Development Code (LDC):

1. The proposal is consistent with the State Comprehensive Plan (Chapter 187, Florida Statutes).
2. The proposal is consistent with the East Central Florida Strategic Policy Plan.
3. The proposal is consistent with the provisions of Chapter 163, Part II, Florida Statutes.
4. The proposal is consistent with the objectives and policies of the City's adopted Growth Management Plan (GMP); particularly, Future Land Use Policy 2.4.4, Figure LU-1, Goal 4 and its associated objectives, policies, and figures.
5. The proposal is consistent with and implements the Southeast Orlando Sector Plan, Chapter 68 of the Land Development Code.
6. The proposal is consistent with the requirements of the Blackfin Shoppes PD.
7. The proposal is compatible with the surrounding development and neighborhood pattern.
8. The proposal will not result in demands on public facilities and services that exceed the capacity of such facilities and services since it is subject to Chapter 59 of the City Code, the Concurrency Management Ordinance.

Staff recommends approval of the Specific Parcel Master Plan subject to the conditions below:

CONDITIONS OF APPROVAL

City Planning

1. GMP & PD AMENDMENT EFFECTIVE

Development of the subject property and issuance of building permits for this master plan shall not be permitted prior to the effective date of GMP amendment (Case #GMP2015-00054) and PD zoning (Case #ZON2015-00057).

2. SUBJECT TO CODES –ZONING

Except as provided herein, the development is subject to all codes and ordinances of the State of Florida, City of Orlando, and all other applicable regulatory agencies.

3. DEVELOPMENT REQUIREMENTS

Except as modified herein, the project shall be developed in accordance with the requirements of the Growth Management Plan, Chapter 68 of the Land Development Code, the Blackfin Shoppes PD, and any other pertinent provisions of the Conventional LDC, and all previous agreements between the City and property owner.

CONDITIONS OF APPROVAL

City Planning (cont.)

4. APPROVAL

Approval of the Specific Parcel Master Plan by City Council shall grant the applicant authority to submit an application for site plan/master plan review for a building permit. The application must be submitted within thirty-six (36) months of approval of the Specific Parcel Master Plan (by the City Council) or the Master Plan shall expire. However, upon written application filed 30 days prior to the expiration date, the Planning Official may renew the Master Plan for one period of up to 12 months providing good cause is shown.

5. DEVELOPMENT PERMITS

As provided by subsection 166.033(5), Florida Statutes, issuance of a development permit by a municipality does not in any way create any right on the part of an applicant to obtain a permit from a state or federal agency and does not create any liability on the part of the municipality for issuance of the permit if the applicant fails to obtain requisite approvals or fulfill the obligations imposed by a state or federal agency or undertakes actions that result in a violation of state or federal law. In accordance with subsection 166.033(5), Florida Statutes, it is hereby made a condition of this permit that all other applicable state or federal permits be obtained before commencement of the development.

6. LANDSCAPING PLAN

The landscaping plan shall be developed in accordance with LDC Chapter 60. A final landscape plan, including plant list, shall be submitted for Appearance Review prior to building permits. Include additional landscaping to make up for amount lost. This must be achieved by calculating tree points as depicted in LDC Chapter 60. A tree survey and tree removal permit is required.

7. APPEARANCE REVIEW

All Buildings and site plans shall be reviewed administratively through Planning Official Determinations prior to submitting for building permits.

8. SIGNAGE

The applicant shall submit a master signage plan prior to the issuance of sign permits.

9. DUMPSTERS

All dumpsters and trash compactors shall be screened with solid walls to match the principal structure. Decorative gates shall be installed to coordinate with principal structure. No dumpsters or mechanical equipment shall face Narcoossee Road.

10. BOLLARDS

Curb cuts along building frontages shall be designed to minimize the need for vertical bollards and "No Parking Signs". Bollards shall be compatible with the materials and colors of the principal structure.

11. DRIVE THRU

For the fast food restaurant, the drive-thru shall be oriented to the side or rear and not face Narcoossee Road. It shall also be reviewed for code compliance as part of the Appearance Review.

12. WETLAND IMPACT

The applicant will have to get the required authorization from the South Florida Water Management District (SFWMD) and satisfy any mitigation requirement from that agency.

13. WETLAND SETBACK

The minimum principal building setback from the retained wetland is 50 feet from the wetland boundary. Currently the SPMP depicts in Lot 2, the grocery store, a setback of 25 feet from wetland boundary. To avoid further wetland impacts, this setback is approved as proposed.

14. STREET FRONTAGE

Street frontage is required for each lot delineated in the master plan. As depicted in the proposed SPMP, Lot 3 does not have street frontage. The applicant shall revise the plan to adjust lot boundaries to allow for adequate street frontage for Lot 3.

15. PARKING

Each phase of development must meet the regulations of the Land Development Code. Parking shall not be constructed on Lot 6 until it is developed. The minimum and maximum parking ratio must be met for each phase of development.

CONDITIONS OF APPROVAL

City Planning (cont.)

16. REINFORCED WALL SECTION

The Reinforced Wall Section shall be revised to show dimensions and proposed curb.

17. REQUIRED LANDSCAPE BUFFER

According to LDC Section 61.312, the landscape area must be 7.5 feet adjacent to rights of way and property lines. This plan will allow an alternative standard to this requirement for the area adjacent to the wetland area against the retaining wall. The applicant shall provide additional interior landscaping to compensate for the lack of perimeter landscaping along the wetland area.

18. PUBLIC WORKS REVIEW

The site plan is subject to Site Engineering, Stormwater and Engineering/Zoning staff review for approval of wetland impact and mitigation through a Planning Official Determination prior to building permits.

Urban Design

1. Appearance review is required prior to permitting for all buildings, via Planning Official determination. All buildings shall incorporate a durable base (non-stucco) material a minimum of 2-ft in height, the required transparency, a three dimensional treatment of the parapets (with a minimum 5-ft return). Architectural continuity between the buildings is required to be incorporated into each building, such that there are not random buildings that are unrelated to each other; this may include, but is now limited to: continuity of materials, similar base and parapet treatments, coordinated landscaping, etc. All other code requirement shall also be incorporated into the design at this required review (e.g. the current elevations provided for the store may not meet the required screening of all mechanical equipment on the roof, which is required to be screened horizontally).
2. Provide 30% window transparency toward Narcoossee Road, and 15% towards the driveway that leads to the high school (which was originally planned as a right of way) on all buildings.
3. Add trees to the peripheral parking lot landscaping on-site within the 7.5-ft landscape strip behind Orange County easements along Narcoossee Road, in order to mitigate for the lack of code required street trees along Narcoossee. Provide a minimum 7.5-ft parkstrip along the high-school entrance drive with street trees, and include the code required parking lot landscaping at a minimum of 7.5-ft beyond the sidewalk.
4. The high school entrance driveway's sidewalk abruptly ends at the project's entranceway to the north. A marked crossing should be provided to accommodate the pedestrian visitors from school activities to the shopping center; it is unrealistic to only provide a cross-walk at Narcoossee and expect a non-direct pedestrian path between these uses. The Village Center designation must provide more direct pedestrian paths between these potentially highly trafficked civic and retail uses.
5. Provide a logical direct walkway between buildings entrances of Lots 1 and 4.
6. Provide a direct walkway, consistent with the requirements of Section 61.314 of the parking code, between the main entrance of the anchor grocer and the sidewalk adjacent to Narcoossee Road. This include a walkway and accompanying landscaping (delete the walkway that currently runs along the northside of the grocery parking lot – this is the code required treatment, and the proposed one is inadequate).
7. Provide additional landscaping along the Lot 2 grocer's northern "sheer wall", with a minimum Bufferyard "A" plantings, as this elevation is exposed to the north.

Transportation Planning

1. CITY SERVICES AND SIDEWALK EASEMENT

If not already recorded, a City Services and Sidewalk Easement shall be granted along the east boundary. The width shall be sufficient to include all parts of the sidewalk which lie outside the Narcoossee Road right-of-way.

Transportation Engineering

1. The proposed improvements to Tyson Road including roadway realignment and changes to signal timing must be approved before any permit is released. The roadway alignment must not create a lane shift for traffic traveling both east and west on Tyson Road as it crosses Narcoossee Road.
2. Improvements to the intersection at Tyson and Narcoossee must include ADA ramps that meet current FDOT standards for design.

CONDITIONS OF APPROVAL

Transportation Engineering (cont.)

3. Sidewalk width along Tyson Road will at a minimum, match existing width of sidewalk on the north side of Tyson Road.
4. A permit from Orange County will be required for the proposed relocation of the driveway on Narcoossee Road.
5. Provide a clear pedestrian sidewalk throughout the project to provide connections for all proposed buildings that are ADA accessible.
6. All sidewalks directly adjacent to traveled ways shall be a minimum 6 feet wide. Any that are perpendicular to parking spaces shall also be a minimum of 6 feet wide or provide a wheel stop.
7. All two way drive aisles shall be a minimum of 24 feet wide.
8. At all project entrances, clear sight distances for drivers and pedestrians shall not be blocked by signs, buildings, building columns, landscaping, or other visual impediments. No structure, fence, wall, or other visual impediment shall obstruct vision between 2 feet and 8 feet in height above street level. The street corner / driveway visibility area shall be shown and noted on construction plans and any future site plan submittals. The applicant shall design the site plan as necessary to comply with the Florida Greenbook and the FDOT Design Standards Index. Sight lines shall be provided on both site plans and landscape plans.
9. The final site plan shall show the location and size of the on-site solid waste compactor(s) / dumpster(s) with concrete pads, and enclosures with doors. The solid waste container(s) shall not be located adjacent to any single family houses or directly adjacent to the public street. Dumpsters shall be located to provide a minimum 50 feet of clear backup space and constructed per Orlando Engineering Standards Manual (ESM) requirements, OR documentation shall be provided from the City's Solid Waste Division indicating curb pick-up or other approved arrangement.

Police

1. CPTED REVIEW

The Orlando Police Department has reviewed the plans for Shoppes at Tyson Place located at 13000 Narcoossee Road., utilizing CPTED (Crime Prevention Through Environmental Design) principles. CPTED emphasizes the proper design and effective use of a created environment to reduce crime and enhance the quality of life. There are four overlapping strategies in CPTED that apply to any development: Natural Surveillance, Natural Access Control, Territorial Reinforcement and Target Hardening. CPTED conditions will be emailed to the client by the Project Manager with the City.

2. NATURAL SURVEILLANCE

Design the site to keep intruders easily observable. This is promoted by features that maximize visibility of people, parking areas and building entrances; doors and windows that look out onto streets and parking areas; pedestrian-friendly sidewalks and street; porches or patios and adequate nighttime lighting.

- Lighting plays a very important role in CPTED. It is crucial that lighting sends the right messages to the public about the safe and appropriate use of space at different times of the day and night.
 - All lighting for this project shall meet or exceed the guidelines in Orlando City Code, Title II, Chapter 63, Part 2M.
 - In order to create a sense of safety, pedestrian-scale lighting should be used in all high-pedestrian traffic areas to include building entrances, parking facility entrances, common areas, parks spaces, courtyards, walkways and service areas.
 - Appropriate lighting should be included in all areas anticipated to be used after-dark.
 - Lighting should not be screened out by landscaping or building structures such as overhangs or awnings.
 - Uniformity of light is crucial to avoid 'dark' spots, especially in parking areas.
 - Any illumination shall not cause a glare or excessive brightness that adversely affects the vision of pedestrians or motor vehicle operators on public or private property.
 - Pedestrian walkways, back lanes and access routes open to public spaces should be lit so that a person with normal vision is able to identify a face from a distance of 30 feet during nighttime hours.
- The use of full cut-off or shielded light fixtures can direct light where it is intended while reducing light trespass, glare, and waste.
- Landscaping is another crucial aspect of CPTED. Trees branches should be kept trimmed to no lower than 6 feet from the ground and shrubs should be kept trimmed to no higher than 30 inches. Avoid conflicts between landscaping and lighting, especially lighting adjacent to canopy trees. Landscaping should not create blind spots or hiding places and should not block/cover windows. Open green spaces should be observable from nearby structures.

INFORMATIONAL COMMENTS

Police (cont.)

- Outdoor furniture placed in common areas is a good way to increase surveillance and encourage positive community interaction. If used, consider furniture designs that encourage stopping and resting but reduce opportunities for potential offenders (i.e. a ribbed design rather than solid and center rails or arm rests to discourage sleeping).
- Bicycle parking (if installed) should be observable from building entrances, securely fastened, not located in remote or low-traffic areas and not hidden behind landscaping.
- Vehicle and pedestrian entrances into the parking facility should be well lit and defined by landscaping, signage and/or architectural design.
- Exterior (non-public) doors should contain 180° viewers/peep holes.
- Windows should look out onto streets, courtyards, and low-traffic areas.
- Since there is typically no natural surveillance around dumpsters or service areas, be sure these areas are well-lit and that lighting is well maintained.
- Office areas should have exterior windows and furniture oriented to improve surveillance of entrances and public areas.
- All sides of the buildings should have windows to observe the walkways, parking areas and driving lanes.
- Advertisements and product displays should not be located in windows. If advertisements must be used, they should be small in size and located in an area that will not interfere with visibility through the windows.
- Public restrooms should be visible from the main customer areas and away from outside exits.

3. NATURAL ACCESS CONTROL

Design the site to decrease crime opportunity by denying access to crime targets and creating in offenders, a perception of risk. This can be accomplished by designing street, walkway, building and parking lot entrances to clearly indicate public routes and discourage access to private areas with structural and landscape elements.

- Public entrances should be clearly defined by walkways, signs, and landscaping. Landscaping used around building entrances should create clear way-finding, be well lit and not block entrances or create ambush points.
- There should be no easy access to the roof of any building.
- Signs located throughout the parking areas should remind users to lock their vehicles and keep valuables out of sight.
- Way-finding located throughout the property should provide clear guidance for authorized users while discouraging potential offenders. Signs should clearly indicate - using words, international symbols, and maps - the location of entrances, restrooms, parks, public or private use routes, emergency telephones, and community activities.
- Signage with hours of operation should be clearly visible at any public entrance.
- The use of traffic calming measures as well as surface and gateway treatments is encouraged to promote safe vehicle speeds, reduce collision frequency and increase the safety and the perception of safety for non-motorized users.
- Walkways throughout the project should be a minimum 6' in width to enhance pedestrian flow.
- Bollards are a good option to consider in key locations to protect pedestrians, life-safety elements, critical utilities and control or direct traffic.

4. TERRITORIAL REINFORCEMENT

Design can create or extend a sphere of influence, where users develop a sense of territorial control, while potential offenders are discouraged. This is promoted by incorporating features that define property lines and distinguish private spaces from public spaces such as; landscape plantings, pavement designs, gateway treatments and CPTED open design (see-through) fences.

- The property should be designed to encourage interaction between authorized users.
- Each building should have an address that is clearly visible from the street and parking areas with numbers a minimum of five-inches high made of non-reflective material.
- Fencing could be used in select areas to add security, delineate property lines, allow transparency for surveillance, be unobtrusive, and create a sense of community. CPTED open style fencing is a good option to consider. Another option is landscape buffers, which include hostile vegetation, to delineate public from private spaces. The fencing and landscape buffer may be used together to further define and control spaces.
- Maintenance is an important aspect of territorial reinforcement. A well-maintained area sends the message that people notice and care about what happens in an area. This in turn discourages vandalism and other crimes.
- To discourage vandalism and unwanted behavior, consider some of the following options for outdoor furniture elements, including low walls, curbs, water features or planter boxes: skate deterrents, arm rests, seat dividers, breaks, bumps, or height variations.

INFORMATIONAL COMMENTS

Police (cont.)

5. *TARGET HARDENING*

This can be accomplished by features that prohibit entry or access such as window locks, dead bolts for doors and interior door hinges.

- Door locks should be located a minimum of 40 inches from adjacent windows.
- Exterior doors should contain 180° viewers/peep holes or small windows made of security glass, interior hinges, single cylinder deadbolt locks with a minimum two-inch throw, metal frames with 3" screws in the strike plates, and be made of solid core material.
- All windows that open should have locks.
- Air conditioner units should be caged and the cages should be securely locked.
- If alarm or security systems are installed, each suite or unique space should be clearly identified with the monitoring center and each business should have a separate system that can be regularly tested and maintained by the occupants. During working hours, commercial alarm systems should be programmed so that a short beep is sounded if an exterior door opens.
- A video surveillance system capable of recording and retrieving an image to assist in offender identification and apprehension is a good option at access points and around the parking areas: especially areas with limited or no natural surveillance. Cameras should be mounted at an optimal height to capture offender identification. "Aiming" down from steep angles often presents challenges to identification.
- Back or service doors not open to the public should be kept locked from the outside at all times. Internal business policy should prohibit the "propping open" of exterior doors.
- The use of tempered, impact resistant, or security glass or security film is encouraged for all large glass doors and windows. If security film is utilized, ensure that the light transmittance of the security film is greater than or equal to the light transmittance of the window's glass.

Additional precautions, such as security alarms and business watch programs, should be discussed with OPD's Crime Prevention Unit Officer Edgar Malave, 407.246.2513.

6. *CONSTRUCTION SITE PROTECTION*

Due to the continued trend of theft of building materials and equipment from construction sites, Orlando Police Department's Crime Prevention Unit strongly recommends that the developer institute the following crime prevention/security measures at this project site:

- Post signs at the site that theft from the site or trespassing on a construction site is a felony under Florida Law and that the developer will prosecute.
- To improve visibility of potential offenders by OPD patrol officers, perimeter lighting should be installed at a minimum of 150 foot intervals and at a height not less than fifteen (15') from the ground. The light source used should have a minimum light output of 2,000 lumens, shall be protected by a vandal resistant cover, and shall be lighted during the hours of darkness.
- In addition to lighting, one of the following physical security measures should be installed:
 - Fencing, not less than six (6') feet in height, which is designed to preclude human intrusion, should be installed along the perimeter boundaries of the site and should be secured with chain and fire department padlocks for emergency vehicle access; post in a clear area, an emergency contact person and phones numbers for after hours, in case of an emergency; or
 - A uniformed security guard should be hired to continually patrol the construction site during the hours when construction work has ceased.
- Valuable construction materials and tools should be protected in a secondary fenced, locked cage.
- Post in a clean, open area, the name and numbers of an emergency contact person for OPD in case of a night-time emergency.

If you have any questions, please call the Crime Prevention Unit, Officer Edgar Malave, 407.246.2513.

Fire

1. Every building constructed shall be accessible to fire department apparatus by way of access roadways with all-weather surface of not less than 20 feet of unobstructed width, adequate roadway turning radius, capable of supporting the imposed loads of fire apparatus, and having a minimum vertical clearance of 13 ft. 6 in. [NFPA 1: 18.2.3.4]
2. TRC fire code review is preliminary in nature, and is intended to expose or prevent evident design deficiencies with State and City Fire Codes. The design will be reviewed in detail for State and City Fire Code compliance at the time of permit application.
3. Be advised that any new construction must adhere to the requirements of the Florida Fire Prevention Code, 2012 Edition, and The City of Orlando Fire Prevention Code.

INFORMATIONAL COMMENTS

Wastewater

This property lies outside the City sewer service area, however this property will be served by City of Orlando for reclaimed water. Wastewater Division review of the building permit plans is required.

OUC

Submit detailed water utility plans to Orlando Utilities Commission Development Services when they have been developed. See our Website for submittal information <http://www.ouc.com/business/water-services>.

Building

Building Plan Review is not applicable to this case at this time. The building design will be reviewed for code compliance during the design development and construction documents phase.

CONTACT INFORMATION

City Planning

For questions regarding City Planning review, please contact Colandra Jones at 407.246.3415 or colandra.jones@cityoforlando.net.

Urban Design

For questions regarding Urban Design plan review, please contact Jason Burton at 407.246.3389 or jason.burton@cityoforlando.net

Transportation Planning

For questions regarding Transportation Planning plan review, please contact John Rhoades at 407.246.2293 or john.rhoades@cityoforlando.net.

Transportation Engineering

For questions regarding Transportation Engineering review, please contact please contact Lauren Torres at 407.246.3322 or lauren.torres@cityoforlando.net.

Police

For questions regarding Orlando Police Department plan review, please contact Audra Nordaby at 407.246.2454 or audra.nordaby@cityoforlando.net.

Fire

For questions regarding Fire plan review, please contact Charles Howard at 407.246.2143 or charles.howard@cityoforlando.net.

Wastewater

For questions regarding Wastewater plan review, please contact Vince Genco at 407.246.3722 or vince.genco@cityoforlando.net.

Building

For questions regarding Building Plan Review issues contact Don Fields at 407.246.2654 or don.fields@cityoforlando.net.

REVIEW/APPROVAL PROCESS—NEXT STEPS

1. SETDRC minutes scheduled for review and approval by City Council.
2. Appearance Review by the Urban Design staff.
3. Building permits.