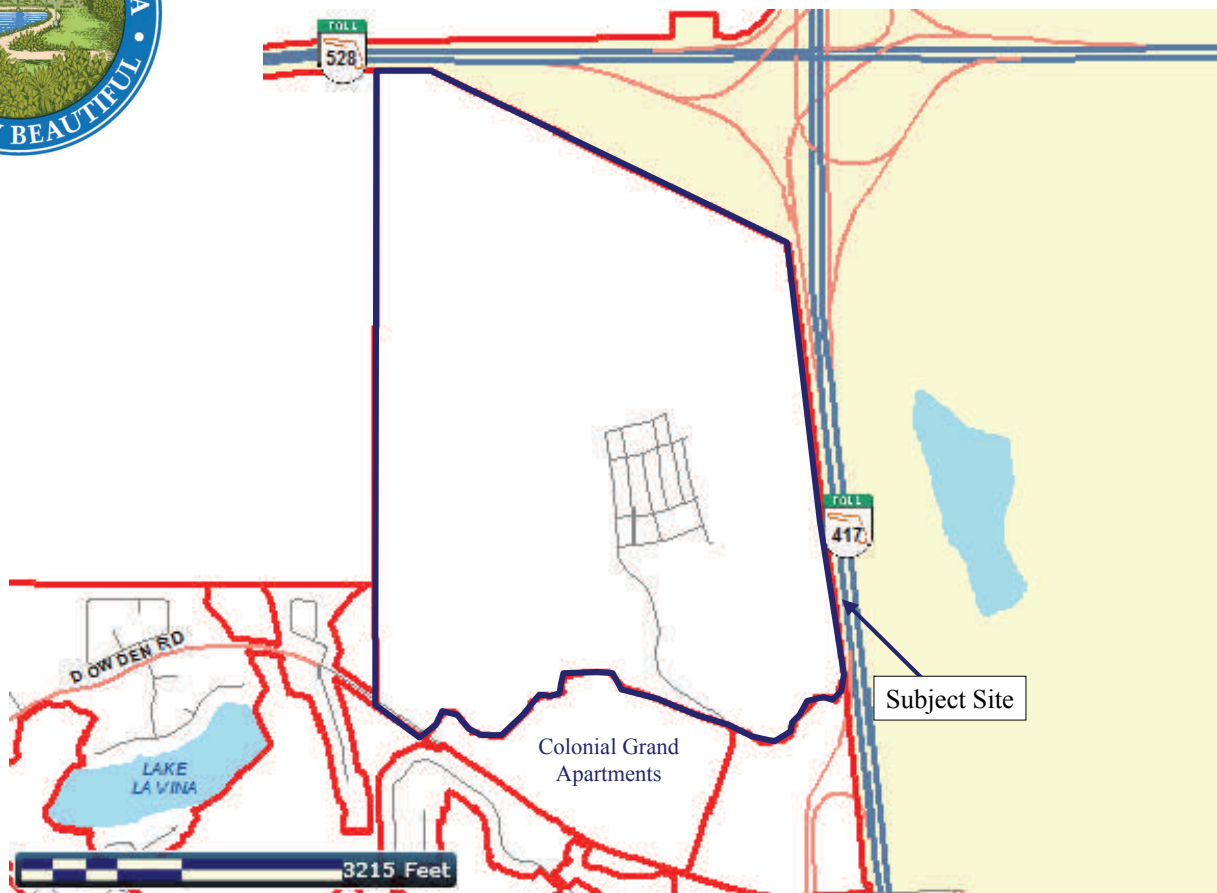




RANDAL PARK PHASES 2-5



Location Map



SUMMARY

Owner/Applicant

Richard Stevens
Mattamy (Jacksonville)
Partnership

Project Planner

Colandra Jones

Property Location: The subject property is located north of Dowden Road, west of Central Florida Greenway (SR 417), south of the Beachline Expressway (SR 528), and east of Narcoossee Road (±461.5 acres, District 1).

Applicant's Request:

Specific Parcel Master Plan (SPMP) approval for Phases 2,3,4 and 5 of the Randal Park residential development. The project is comprised of 558 residential units with 4 different lot types including 335 detached single family units and 223 townhome units.

Staff's Recommendation:

Approval of the request, subject to the conditions in this report.

Addendum:

This addendum is to revise the Transportation Engineering comments for MPL2013-00002.

INFORMATIONAL COMMENTS

Transportation Engineering

2. SHEET NUMBER C006

- ~~The intersection of "Street I" at "Street H" near the school site shall align with the school driveway.~~
- ~~The alley for lots 122-131 shall be one-way SB to prevent vehicles from entering the alley so close to Randal Park Blvd.~~
- Where "Street I" intersects "Street H" across from the elementary school property, a raised, triangular island shall be constructed to prohibit southbound left turns onto "Street H" and the sidewalk on the north side of "Street H" shall be realigned so the walking path avoids the raised island.
- The uncontrolled crosswalk on "Street H" east of the school driveway shall be relocated east of and outside the alleyway curb cut.
- There shall be a signed no parking zone on both sides of "Street H" adjacent to the elementary school property and on the west side of "Street H" adjacent to the open space.
- The alleyways serving lots 118-130 and lots 131-152 shall be one-way southbound and one-way northbound, respectively.

4. SHEET NUMBER C008

- There is no need for a mid-block crossing on "Street Q" at lot 563.
- ~~According to the PD, Randal Park Blvd or "Street P" was to connect all the way to "Street O" by way of "Street U".~~
- The intersections of "Streets S & Q" at "Streets O & T" are too close to each other.

5. SHEET NUMBER C011

~~"Street A" shall be built with Typical Section A-Spine Road since it will connect out to the future Econlockhatchee Trail extension. The parking lanes shown in Typical Sections A, B, and D shall have marked parking spaces and "bulb-outs" shall be provided within the "no parking" zones near intersections."~~

6. SHEET NUMBER C013

- The gutter pan shall not be included in the width of any traveled way.
- ~~At the most, only 1' of the gutter pan shall be accounted for when measuring parking stall/lane widths. Do not measure from the face of the curb.~~
- Marked parking lanes or parking stalls shall be at least 8' wide.
- Are the parking lanes on Typical Sections B, D, E, & F to be marked? If not, will parking be allowed on both sides? If not, "No Parking" signs shall be installed by the developer according to City Sign Shop specifications.
- Is parking allowed on Typical Sections C & G? If not, "No Parking" signs shall be installed by the developer according to City Sign Shop specifications.
- Any roadway with an asphalt width less than 24' shall have parking restricted on at least one side through the use of "No Parking" signs.
- What is the clear zone between the trail and the drop-off in Typical Section J?
- Since the alleys have center drainage, consider the use of a center concrete gutter.

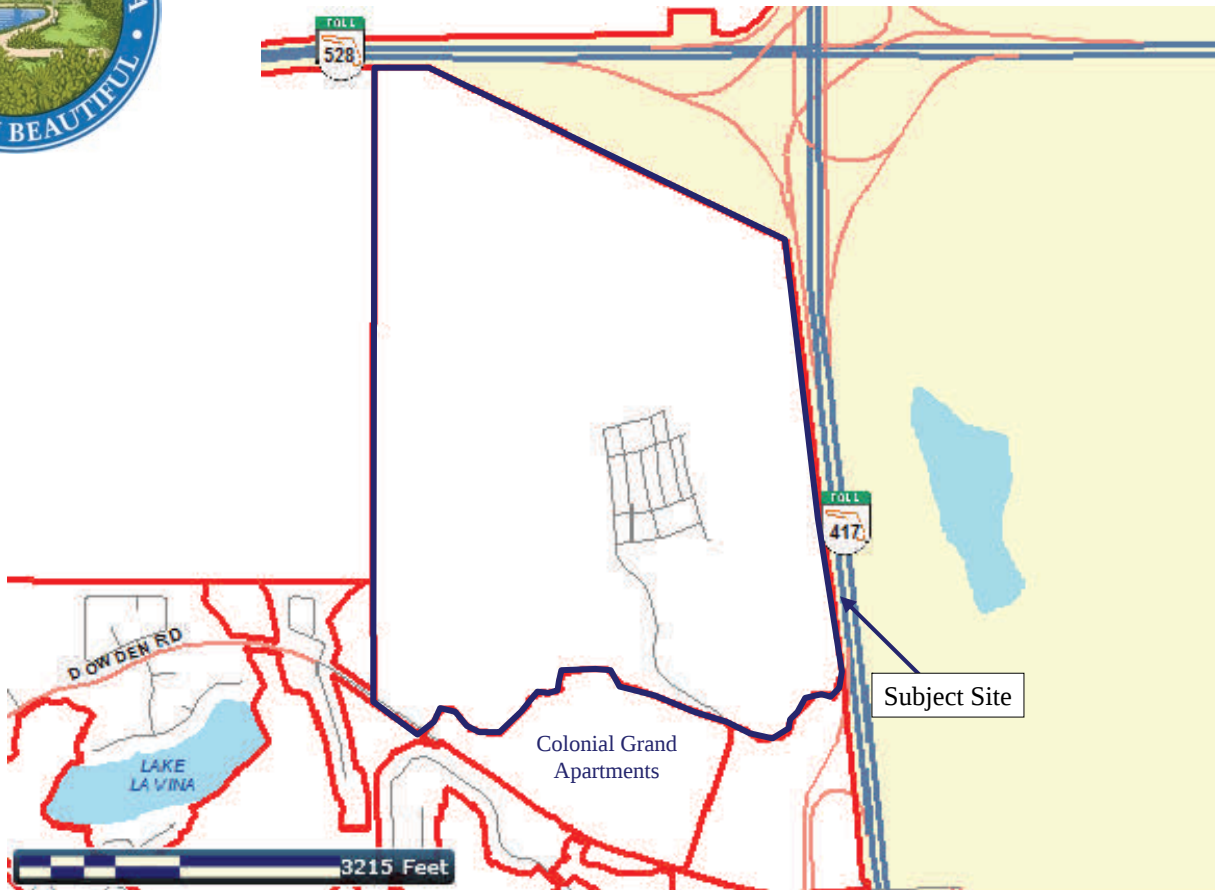
8. ROAD TERMINATION

- Where a roadway stub to a future development is shown, the plans shall have a temporary roadway termination that maintains driveway access to all lots and preserves the road right-of-way for the future connection.
- Any road terminations shall be designed according to City Code and the Engineering Standards Manual.
- In the event that a road termination cannot be constructed within the typical section right-of-way, an easement shall be provided by Randal Park for the road termination until the future connection is completed.

*The underline items indicates new language and the strike-through items indicates language to be deleted.



RANDAL PARK PHASES 2-5



Location Map



SUMMARY

Owner/Applicant

Richard Stevens
Mattamy (Jacksonville)
Partnership

Project Planner

Colandra Jones

Property Location: The subject property is located north of Dowden Road, west of Central Florida Greenway (SR 417), south of the Beachline Expressway (SR 528), and east of Narcoossee Road (±461.5 acres, District 1).

Applicant's Request:

Specific Parcel Master Plan (SPMP) approval for Phases 2,3,4 and 5 of the Randal Park residential development. The project is comprised of 558 residential units with 4 different lot types including 335 detached single family units and 223 townhome units.

Staff's Recommendation:

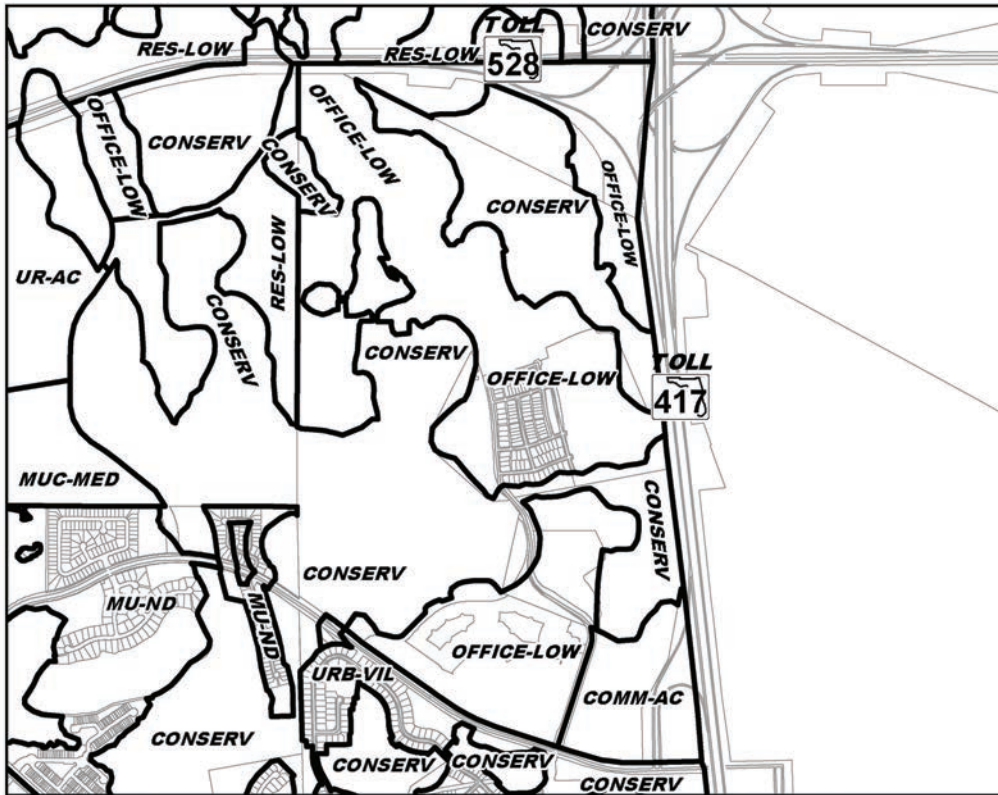
Approval of the request, subject to the conditions in this report.

Public Comment

Courtesy notices were mailed to property owners within 300 ft. of the subject property on March 8, 2013. As of the published date of this report, staff has not received any comments from the public concerning this request.

Updated: March 11, 2013

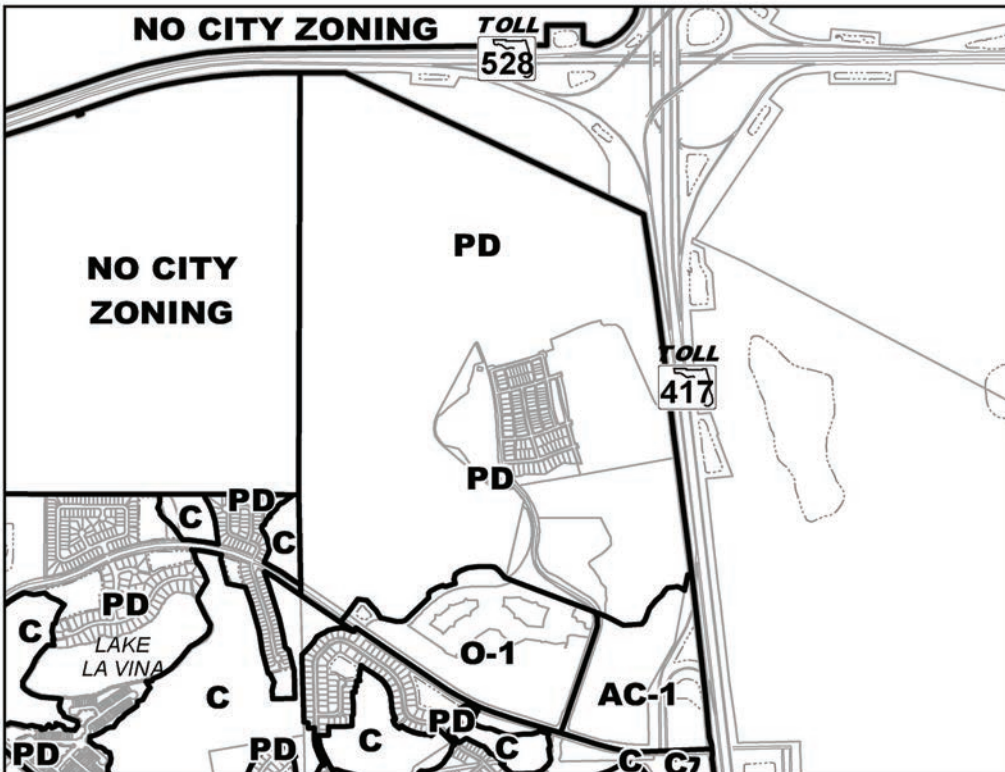
FUTURE LAND USE MAP



Future Land Use - Existing MPL2013-00002



ZONING MAP



Zoning - Existing MPL2013-00002



PROJECT ANALYSIS

Project Description

The subject site is generally located north of Dowden Road, west of Central Florida Greenway (SR 417), south of the Beachline Expressway (SR 528), and east of Narcoossee Road and is approximately 461.5 acres. Of this 461.5 acres, approximately 285.9 acres are preserved wetlands/conservation areas. Therefore, approximately 175.6 acres is actually developable. The site is currently designated Office Low Intensity on the Official Future Land Use Map and is zoned PD. The site is within the Randal Park PD.

The applicant is proposing the remaining development of Randal Park residential neighborhood which is comprised of 558 dwelling units. The SPMP depicts 4 different lot types, including a mix of front-loaded and rear-loaded lots. It proposes 335 detached single family units and 223 townhome units. In addition to the residential uses, the project also contains areas of open space, which totals approximately 12.79 acres of open space, and a bicycle/trail.

Previous Actions:

- August 2010—The MPB approved the request to abandon the 2006 Planned Development (PD) and recommended to adopt a new PD for residential development (ZON2010-00028). The MPB also approved an overall Master Plan for office, commercial and residential development (MPL2010-00013).
- December 2010—The MPB approved the Randal Park Phase 1 SPMP to allow for a residential development comprised of 256 dwelling units (Case #MPL2010-00016).
- July 2012—The MPB approved the Randal Park Clubhouse which is a recreation area that will serve as a community park with amenities including a clubhouse, swimming pool and a playground.

Project Context

The subject property is located within the Randal Park PD, a community located in the southeast section of Orlando. The property is currently vacant. Surrounding future land use designations include Residential Low Intensity and Conservation to the north and west, Office Low Intensity and Community Activity Center to the south and Orange County Planned Development designation to the east. Existing uses include Vista Park residential land to the north which is currently vacant, Orange County Innovation Place DRI to the east which is currently vacant, Bal Bay residential to the west which is also vacant, and the Randal Park Elementary School and Colonial Grand Apartments development to the south. Both developments are currently under construction. The proposed residential development would be compatible with the surrounding existing and planned residential uses.

Table 1—Project Context

	Future Land Use	Zoning	Surrounding Use
North	Residential Low Intensity & Conservation	No City Zoning	Vista Park Residential Neighborhood (Vacant Land)
East	Planned Development (Orange County)	PD (Orange County)	Innovation Place (Vacant Land)
South	Office Low Intensity & Community Activity Center	O-1 & AC-1	Elementary School & Colonial Grand Apartments (both under construction), Vacant Activity Center Land
West	Residential Low Intensity & Conservation	No City Zoning	Bal Bay (Vacant Land)

Conformance with the GMP

As indicated in Figure LU-1 of the Future Land Use Element, residential uses are allowed under the Office Low Intensity future land use designation. The maximum density in this designation is 21 du/ac and it has no minimum density. The proposed use (residential) and density (558 dwelling units on 461.5 acres = 1.21 du/ac) are consistent with the adopted future land use designation.

The Randal Park PD, adopted by City Council on October 18, 2010, is consistent with GMP Subarea Policy S.35.6. This subarea policy provided guiding principles for the overall development of the 683-acre property that makes up the Randal Park PD. The subarea policy addresses various development criteria as it relates to land use, park and recreation facilities, transportation, and school capacity. Subarea Policy S.35.6 also address the review process of individual development sites within the overall master plan of the Randal Park development. According to the subarea policy, *“Each individual development site within the overall master plan shall be reviewed by the Municipal Planning Board as part of a specific parcel master plan that addresses building layout, parking, and other site planning issues as identified in the Land Development Code.”*

Conformance with the LDC

The proposed project is within the Randal Park PD which is primarily a residential development. According to Section 1.7 of the Randal Park PD, adopted by City Council on October 18, 2010, “A specific parcel master plan (SPMP) is required for each phase of development. The SPMP submittal shall be consistent with the master plan requirements of City Code, subject to review and approval by the MPB, and subject to final site plan approval by the City Planning Division. Development standards differing from those contained in the ordinance may be approved during the SPMP review and approval process...”

The proposed project is comprised of 558 residential units. The preliminary phasing plan identifies four phases. Phase 1 of the development, which was approved for 256 single family units, was approved by MPB in 2010. That phase is currently under construction for a total of 239 single family units. Phase 2 consists of 72 single family units and 45 townhome units. Phase 3 consists of 6 single family units and 43 townhome units. Phase 4 consists of 91 single family units and 95 townhome units. Phase 5 consists of 166 single family units and 40 townhome units.

Section 2 of the Randal Park PD describes the residential building standards. It describes the entire development as a diverse residential community with a variety of housing choices, interconnected and walkable residential blocks, and be organized around community parks and amenities. Housing types may vary by lot size and form. These criteria are intended to insure that homes integrate well with each other and share designs which make neighborhood streets safe and enjoyable to walk along. According to condition 1.14 of the Randal Park PD, lot sizes and patterns within a neighborhood shall be varied to avoid monotonous streetscapes and provide a diverse range of housing types. Consistent with this condition, the SPMP proposes seven various lot types which includes a mix of front-loaded and rear-loaded lots. These lot types and the amount proposed for each phase is found in the chart below. In addition to the residential uses, the project also contains neighborhood level parks and open space.

Table 2—Lot Summary				
Lot Type	Phase 2	Phase 3	Phase 4	Phase 5
40' x 110' (Rear Load)	25	2	27	6
50' x 110' (Front Load)	25	2	41	78
60' x 110' (Front Load)	22	2	23	82
Townhome	45	43	95	40
Total	117	49	186	206

Condition 1.16 of the PD depicts a table that outlines the residential types allowed within the development. A portion of this table, as it pertains to the proposed SPMP development, is depicted below with the residential lot type standard, the minimum and maximum allowed in each type, and the number of residential types proposed in the SPMP. It shows the number of units according to PD types for the existing Phase 1 which were approved and the number of units by type proposed in Phases 2-5. The total dwelling units for all housing types allowed in the PD is 1,400 dwelling units. With all phases of the Randal Park development (Phases 1-5) the residential total is up to 797 dwelling units. The existing and proposed units are consistent with the minimum and maximum amount allowed by the PD.

Table 3—Number of Each Unit Type					
PD Residential Unit Type	PD Minimum	PD Maximum	Existing Phase 1	Proposed Phases 2-5	Total
Single family detached front-loaded, 40 to 49.99 foot lot width	0 du	200 du	41 du	0 du	41 du
Single family detached front-loaded, 50 foot or more lot width	100 du	1,200 du	112 du	275 du	387 du
Single family detached rear-loaded, 30 foot or more lot width	100 du	1,300 du	86 du	60 du	146 du
Townhome	0 du	600 du	0 du	223 du	223 du

Condition 1.14.2 of the PD states: *“If less than 100 units of the same building type are proposed, at least three distinct models with at least three alternate elevations for each model must be provided. If front-loaded lots are proposed, the models must include a mixture of projecting and non-projecting garages.”* There are 60 rear loaded lots proposed for this SPMP. For this unit type, 3 models with 3 elevations are required. For each of the other two single family unit types, 4 models with 3 elevations are required. Front-loaded lots must depict a mixture of projecting and non-projecting garages.

Residential Development Standards. According to the Randal Park PD, condition 1.15 provides guidelines for typical development standards. The SPMP process is intended to establish the actual dimensions for each lot and product type. Below is the table, as it pertains to the proposed development, that outlines the typical development standards for single family front-loaded and rear-loaded lots and townhomes as stated in the PD.

Table 4—Randal Park PD Typical Development Standards

Lot Type	Lot Width	Lot Depth	Minimum Street Frontage	Front Yard Setback	Rear Yard Setback	Side Yard Setback	Street Side Yard Setback	Maximum Building Height	Maximum ISR
Single-Family (Front Load)	40' - 70'	100' - 125'	25'	20'	15'	5'	15'	3 stories	75%
Single-Family (Rear Load)	30' - 50'	100' - 125'	25'	16'	5' or >16'	5'	15'	3 stories	75%
Townhome (Rear Load)	20' - 30'	100' - 125'	20'	16'	5' or >16'	0'	15'	3 stories	85%

The chart below is the proposed SPMP development standards for each lot type. All of the proposed development standards are consistent with the adopted PD standards.

Table 5—SPMP Minimum Building Standards

Lot Type	Lot Width	Lot Depth	Primary Building Setback	Front Porch Setback	Rear Yard Setback	Side Yard Setback	Garage Setback	Alley Tract
40' Lot Single-Family (Rear Load)	40'	110'	16'	10'	5' or >16'	5'	N/A	20'
50' Lot Single-Family (Front Load)	50'	110'	20'	10'	15'	5'	24'	N/A
60' Lot Single-Family (Front Load)	60'	110'	20'	10'	15'	5'	24'	N/A
Townhome	20'	110'	16'	10'	5' or >16'	0'	N/A	20'

Transportation. According to the Randal Park PD, Section 3 provides guidelines for transportation needs. The applicant has submitted typical road cross-sections for its Neighborhood Streets and Alleys. The roadway cross-sections, which are found on page 14 and 15 of this report, shows a variety of sections which meet or exceed the PD requirements of 7 foot wide parkways, 5 foot sidewalks, 8 foot wide on-street parking spaces, and 20 foot dedicated two-way alley tracts.

There are two different alley types of cross sections. There is a one-way alley cross-section which has a 20 foot right-of-way a 10 foot travel lane and 4 foot landscaping buffers. The other one-way alley cross-section has a 30 foot right-of-way with an 18 foot travel lane and 5 foot landscaping buffers.

According to the Transportation condition 3.2 in the Randal Park PD, *“The Project’s street network must provide and maintain a minimum 1.4 links-to-nodes connectivity index for each SPMP and for the Project in aggregate.”* A simple measure of connectivity is the number of street links divided by the number of nodes or link ends. The more links relative to nodes, the more connectivity. The Randal Park Phase 2-5 SPMP proposes 38 links and 29 nodes, thus having a connectivity index of 1.31 which is below the minimum required in the PD. Because this SPMP is part of a larger PD with a variety of housing types, a cumulative links-to-nodes analysis is performed. With the street network already planned for Phase1, the cumulative total for all five phases include 64 links and 47 nodes for an overall index value of 1.4, which meets the connectivity requirement.

Open Space. According to General condition 1.13 of the Randal Park PD, “At least ten acres of parks, open space, or recreational areas must be provided within the Project and must be publically accessible. At least half of this minimum requirement must be approved by SPMP by the time half of the residential land within the Project is approved by SPMP. Where feasible, rear yards must not be located along the perimeter of a park unless the intent is to create a linear park or trail connecting natural systems and a series of residential lots.” The proposed SPMP depicts three Park Tracts. One, located in Phase 2, is 2.20 acres in size. The second park tract is in Phase 4 and is 0.62 acres in size. And finally, the last park tract is in Phase 5 and is 0.90 acres. In addition to the Park Tracts, there are 14 small pockets of open space areas throughout the development. All park and open space tracts total 10.68 acres. This proposed park land and open space exceeds the adopted level of service (LOS) standards. For neighborhood parks, the LOS unit equivalent of acres per residential unit is 0.0017. The proposed open space equates to 0.019 acres per residential unit, thus exceeding the adopted LOS. For the overall development, Phase 1 has 6.79 acres of open space and Phases 2-5 has 10.68 acres of open space which equals to 17.47 total acres of open space. This exceeds the minimum amount of open space required by the PD by 7.47 acres. These parks will be privately owned and maintained.

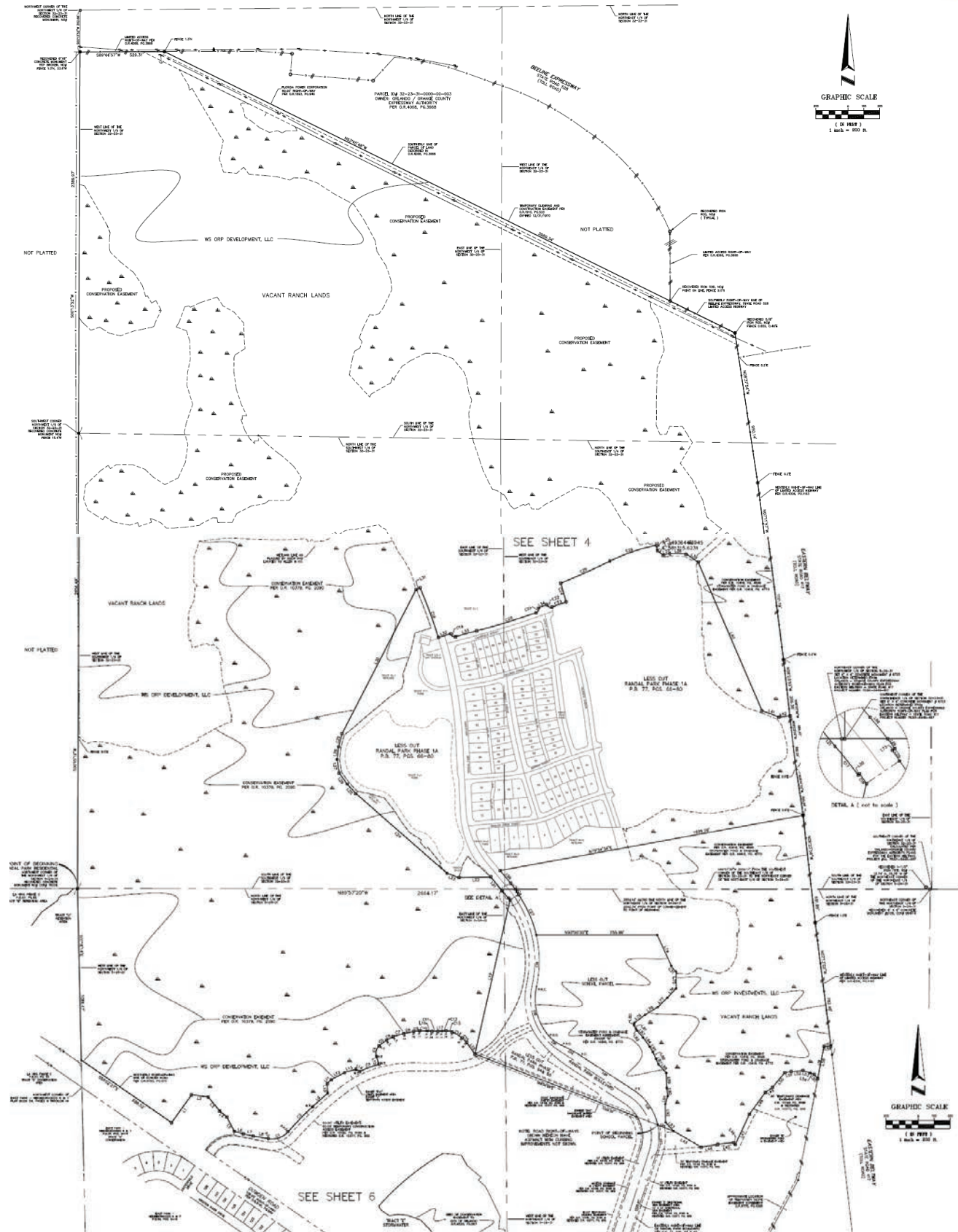
AERIAL PHOTO



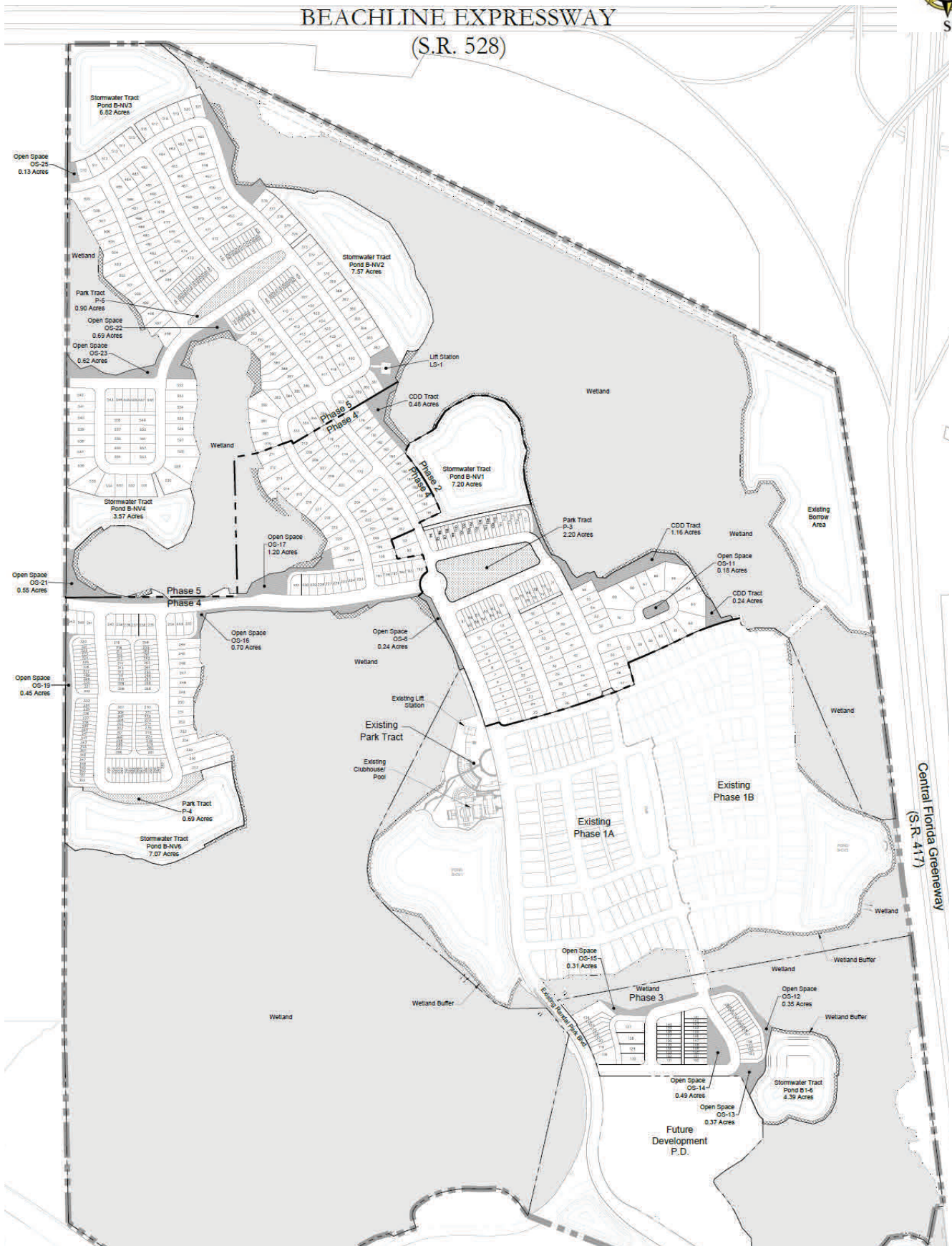
SITE PHOTOS



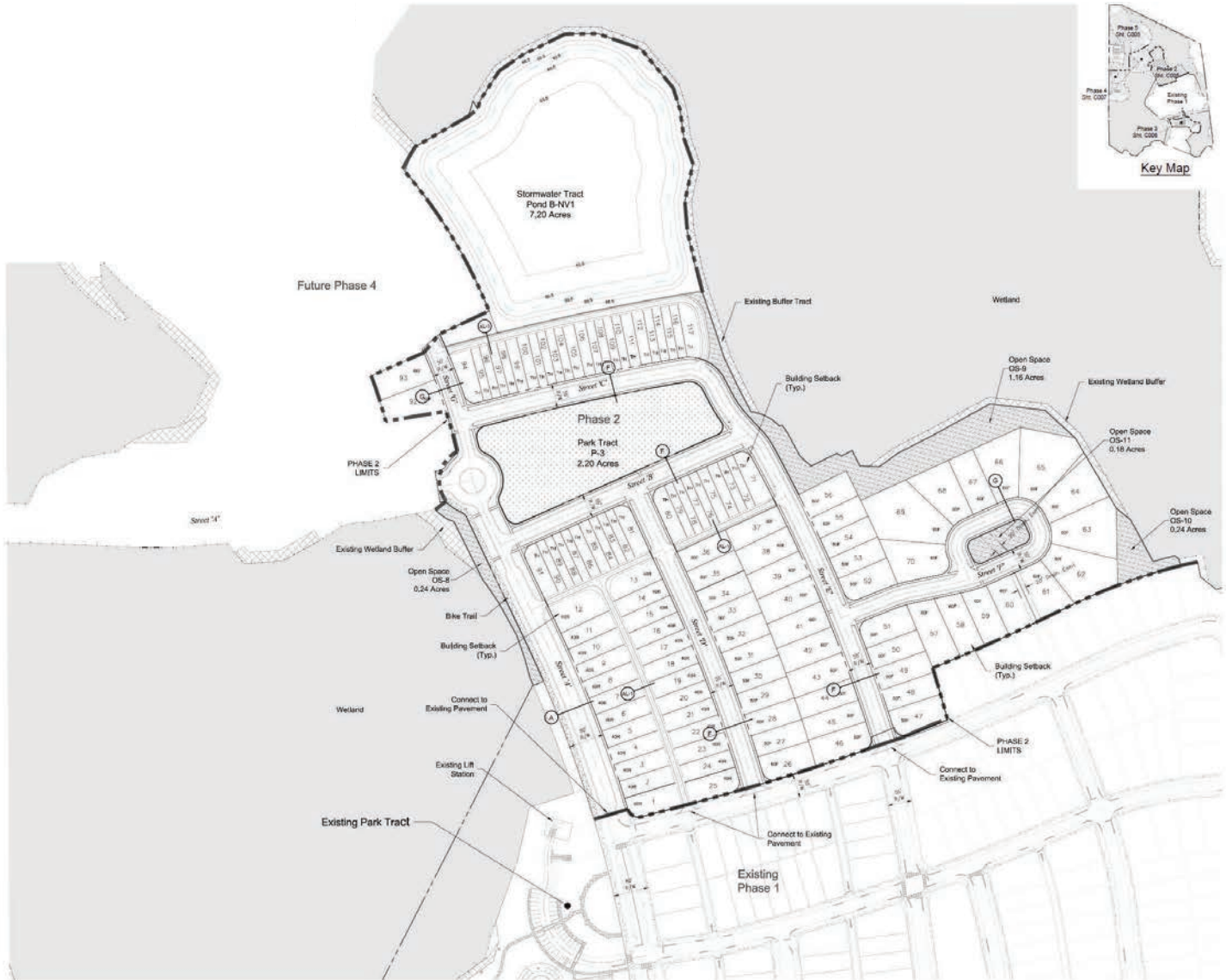
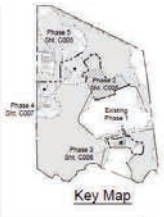
BOUNDARY & TOPOGRAPHIC SURVEY



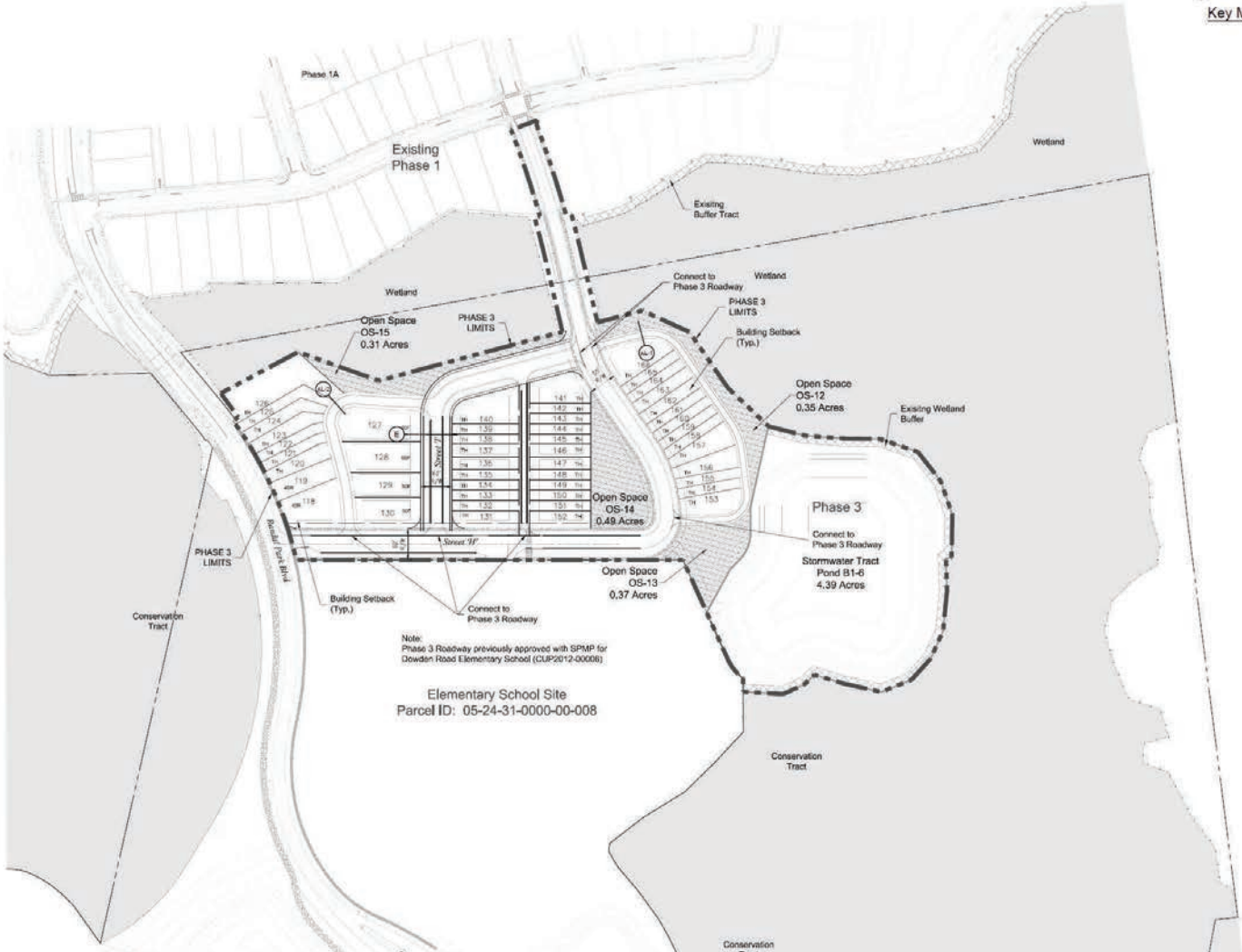
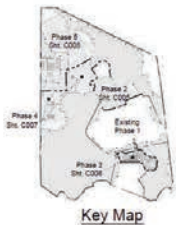
OVERALL DEVELOPMENT PLAN



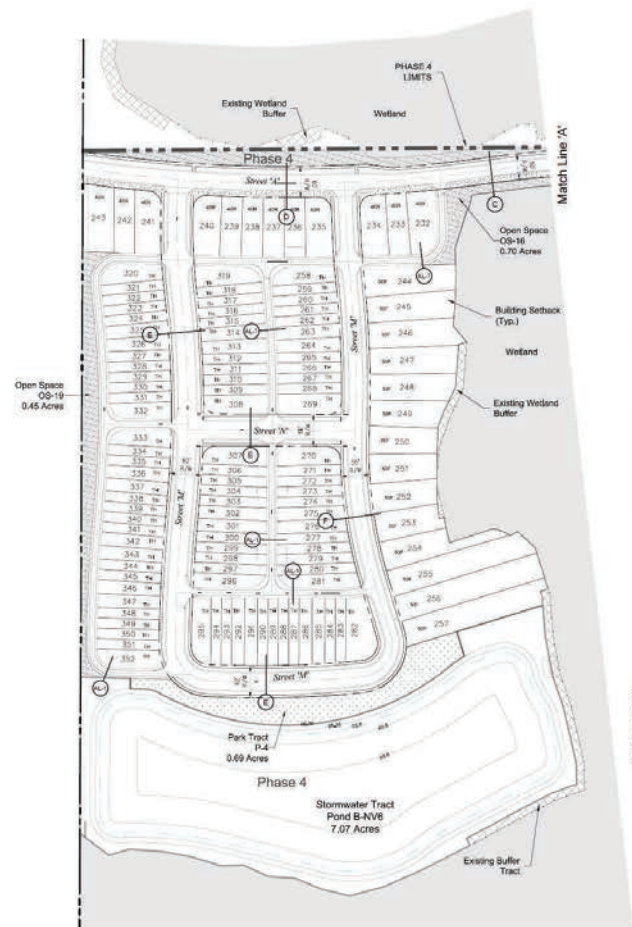
DEVELOPMENT PLAN – PHASE 2



DEVELOPMENT PLAN—PHASE 3



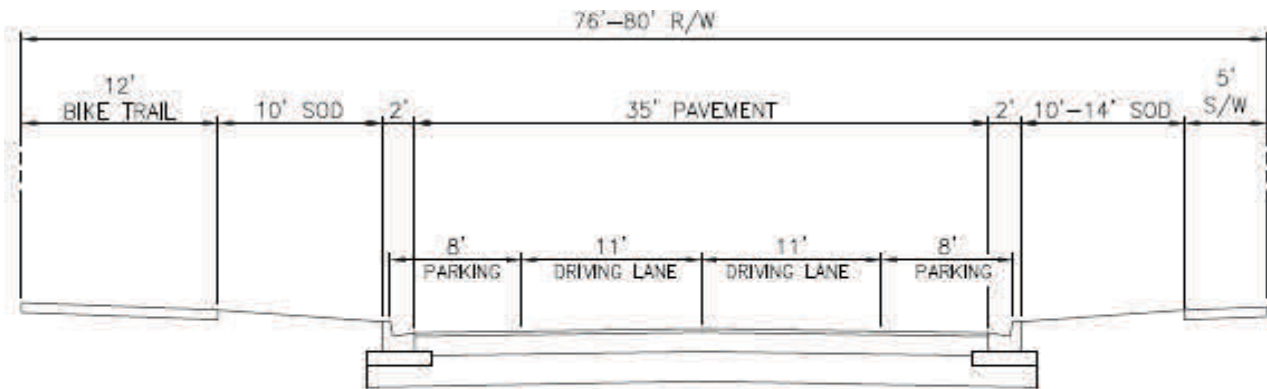
DEVELOPMENT PLAN—PHASE 4



DEVELOPMENT PLAN—PHASE 5

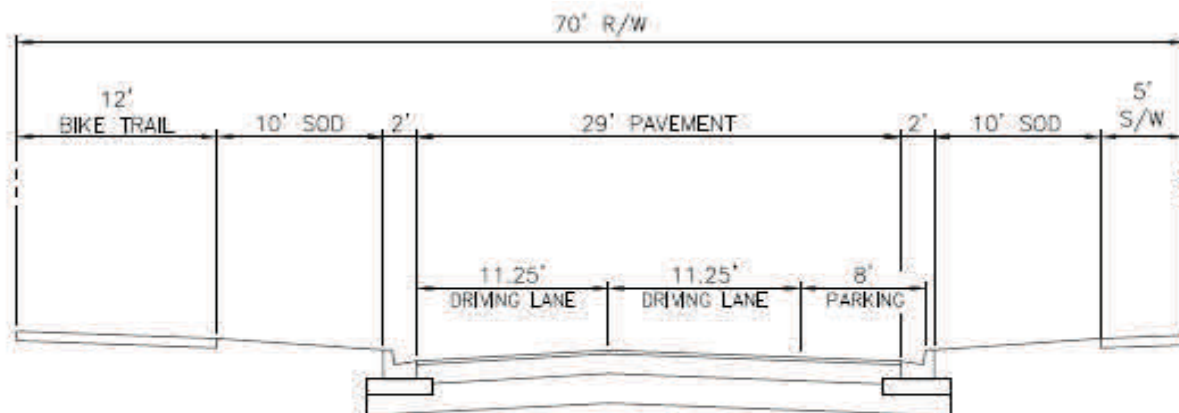


TYPICAL ROAD CROSS SECTIONS

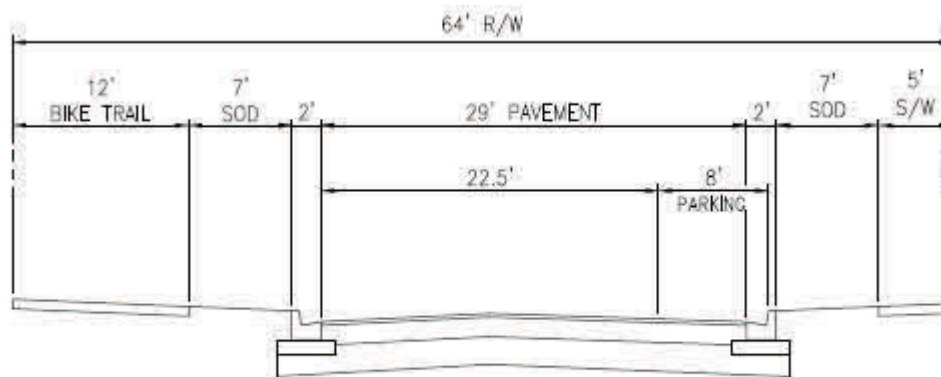


NOTE:
BIKE SHARROW MARKING ARE REQUIRED
ON BOTH SIDES OF THE STREET

A TYPICAL 76'-80' R/W SECTION SPLINE ROAD N.T.S.

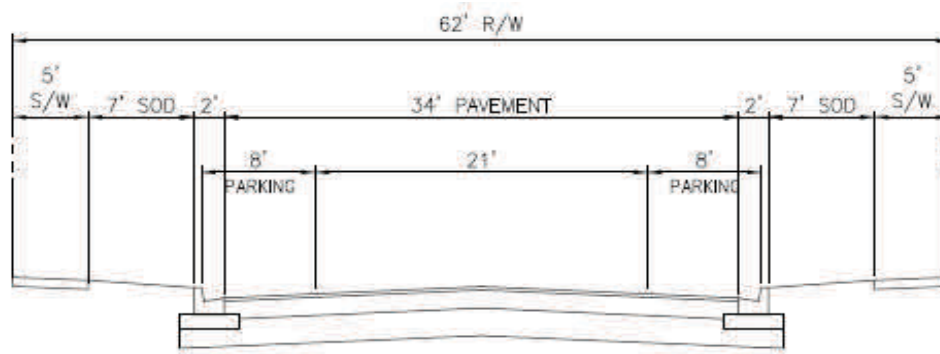


B TYPICAL 70' R/W SECTION PARKING - ONE SIDE N.T.S.

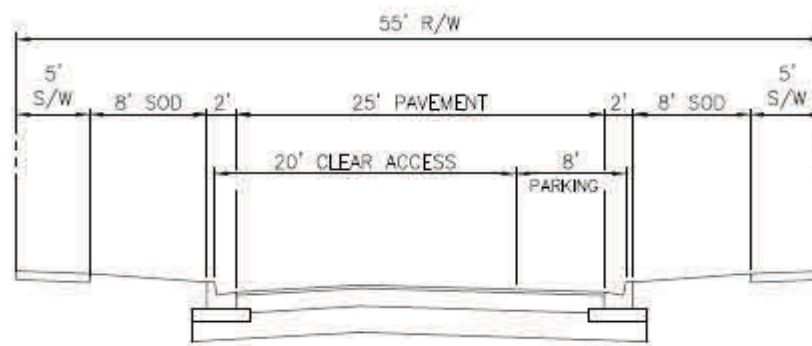


D TYPICAL 60' R/W SECTION PARKING - ONE SIDE N.T.S.

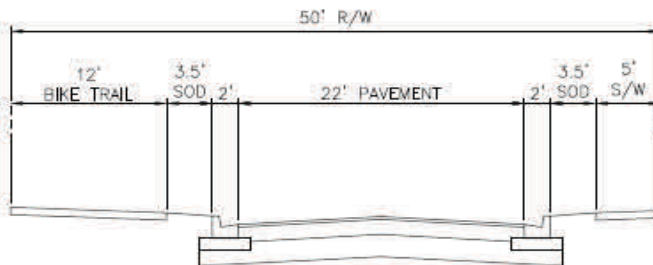
TYPICAL ROAD CROSS SECTIONS



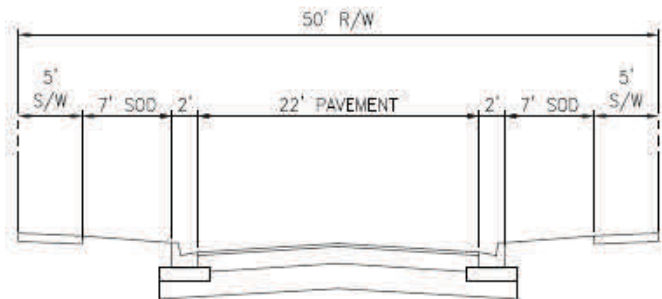
E TYPICAL 62' R/W SECTION PARKING - BOTH SIDES N.T.S.



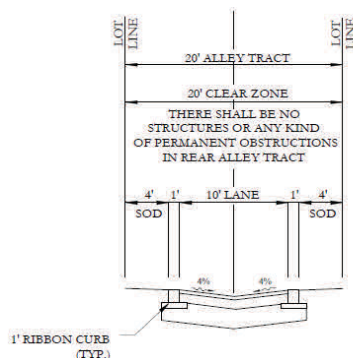
F TYPICAL 55' R/W SECTION PARKING - ONE SIDE N.T.S.



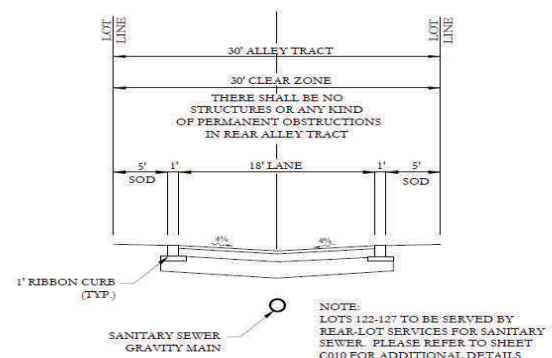
C TYPICAL 50' R/W SECTION N.T.S.



G TYPICAL 50' R/W SECTION N.T.S.

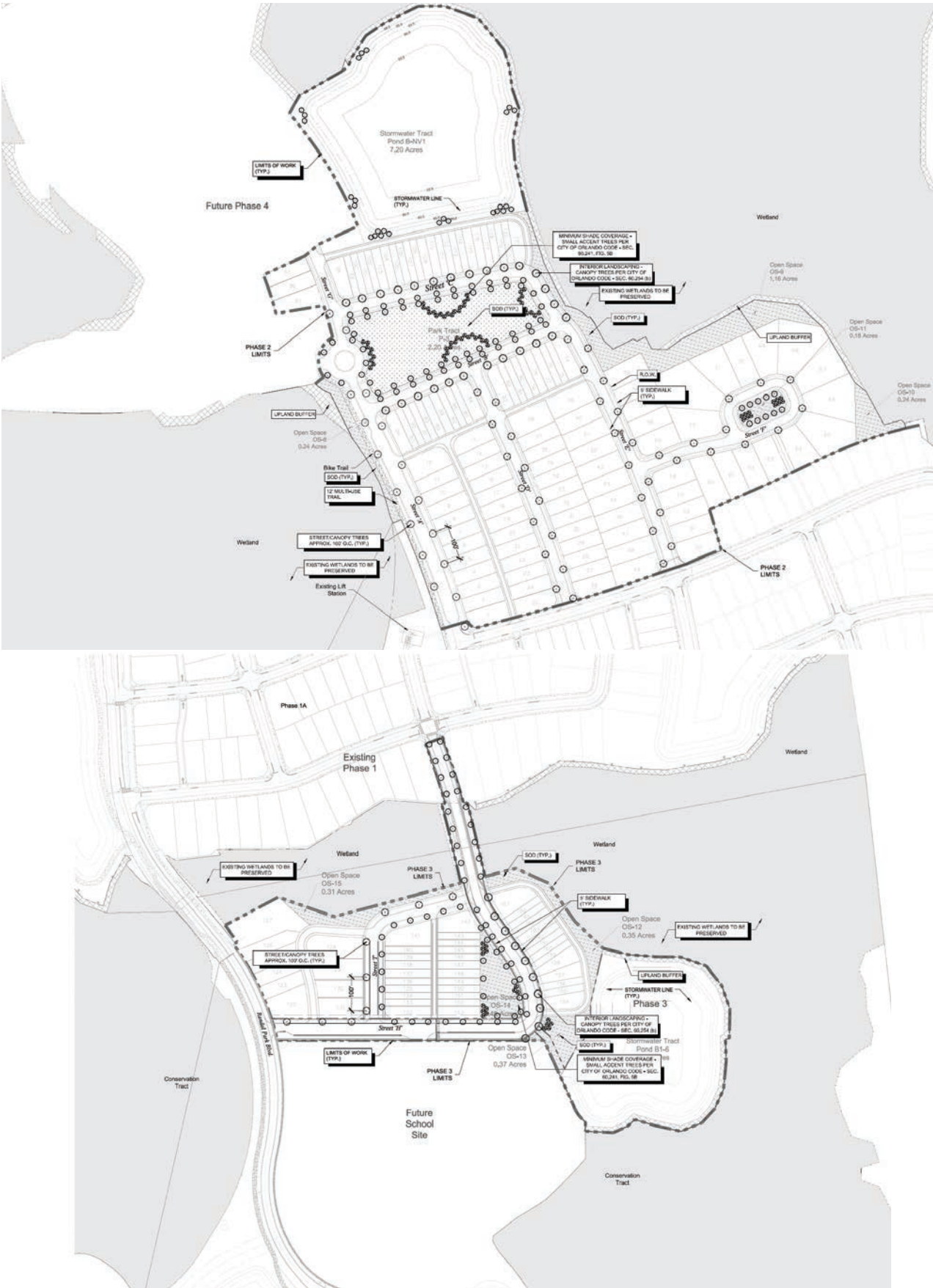


TYPICAL ONE-WAY ALLEY SECTION: AL-1

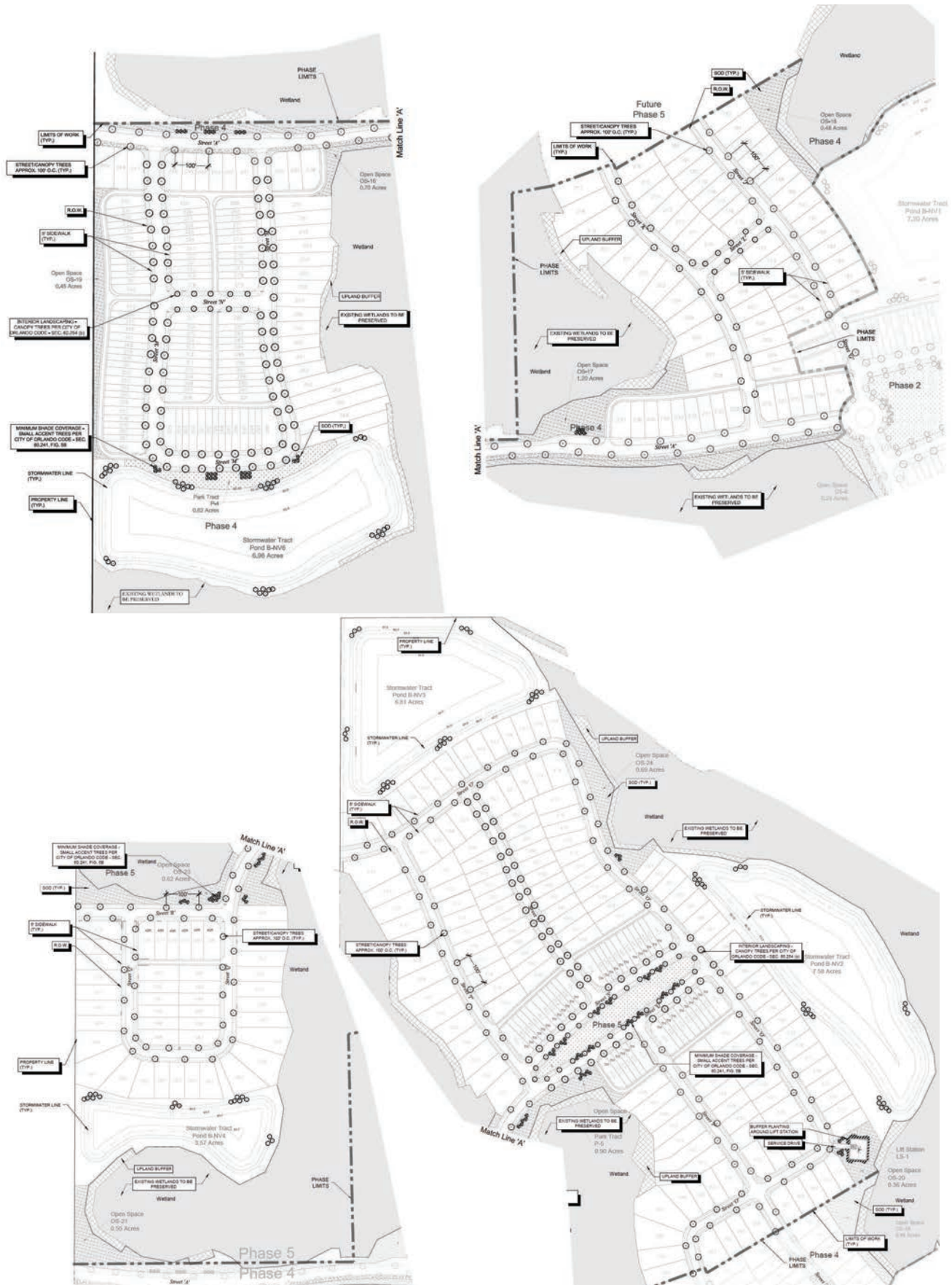


TYPICAL ONE-WAY ALLEY SECTION: AL-2

LANDSCAPE PLAN



LANDSCAPE PLAN



FINDINGS

Subject to the conditions contained herein, the proposal is consistent with the requirements for approval of the proposed Specific Parcel Master Plan applications contained in Section 65.336 of the Land Development Code (LDC):

1. The proposal is consistent with the State Comprehensive Plan (Chapter 187, Florida Statutes).
2. The proposal is consistent with the East Central Florida Strategic Policy Plan.
3. The proposal is consistent with the provisions of Chapter 163, Part II, Florida Statutes.
4. The proposal is consistent with the objectives and policies of the City's adopted Growth Management Plan (GMP); particularly, Future Land Use Subarea Policy S.35.6.
5. The proposal is consistent with the purpose and intent of the requirements of the Land Development Code.
6. The proposal is consistent with the requirements of the Randal Park PD.
7. The proposal is compatible with the surrounding development and neighborhood pattern.
8. The proposal will not result in demands on public facilities and services that exceed the capacity of such facilities and services since it is subject to Chapter 59 of the City Code, the Concurrency Management Ordinance.

Staff recommends approval of the Specific Parcel Master Plan subject to the conditions below:

CONDITIONS OF APPROVAL

City Planning

1. LAND USE AND ZONING

Development of the property shall be consistent with the provisions of GMP Subarea Policy S.35.6, the allowed uses and development standards of the Office Low Intensity future land use designation and the Randal Park PD zoning ordinance.

2. GENERAL CODE COMPLIANCE

Except as provided herein, the development is subject to all codes and ordinances of the State of Florida, City of Orlando, and all other applicable regulatory agencies.

3. EXPIRATION OF THE MASTER PLAN

Upon approval of the Master Plan by City Council, a building permit shall be obtained for the work requiring the master plan within two years of the master plan approval. If the applicant does not receive the building permit within two years, then the master plan is no longer valid and the applicant must reapply for a master plan if the applicant wishes to proceed with a development requiring a master plan. If the building permit for the work requiring the master plan expires before a certificate of occupancy or certificate of completion is issued for the work requiring the master plan, then the master plan is no longer valid and the applicant must reapply for a master plan if the applicant wishes to proceed with a development requiring a master plan. However, the Planning Official may extend this time limit for one period of up to 12 months for good cause shown upon written application filed 30 days prior to the expiration date.

4. MINOR MODIFICATIONS

Following the SPMP approval and before a building permit is issued, minor modifications to the quantifiable standards of the adopted SPMP may be approved by the Planning Official or his designee. Such modifications may not exceed 20% of the adopted numeric standard. The resulting standard must be consistent with the GMP and must be compatible with surrounding development. After a certificate of occupancy is issued for the respective building, zoning variances and modification of standards may only be approved pursuant to the procedures set forth in Part 2J and Part 2F, Chapter 65, Orlando City Code, respectively.

5. APPEARANCE REVIEW

Before submitting for a first building permit for a particular housing model, builders shall request an appearance review from the City Planning Division through a Planning Official Determination to ensure that the elevations are consistent with the Randal Park PD and the final SPMP conditions of approval.

6. ELEVATIONS

As per the Randal Park PD, since less than 100 units of the same building type are proposed, for front loaded lots less than 50' wide at least three (3) distinct models with at least three alternate elevations for each such model must be provided. For front-loaded lots greater than 50' wide, and for rear loaded lots, at least 4 distinct models with 3 alternate elevations are required. For front-loaded lots, the models must include a mixture of projecting and non-projecting garages. All elevations must conform to the requirements listed in Part 2 of the Randal Park PD entitled, "Design and architecture conditions".

CONDITIONS OF APPROVAL

City Planning (cont.)

7. *BUILDING HEIGHT*

As per the Randal Park PD, the maximum single family residential building height shall not exceed three (3) stories in height.

8. *IMPERVIOUS SURFACE RATIO*

As per the Randal Park PD, the maximum ISR permitted for the single family detached front and rear load shall not exceed 75%.

9. *SCHOOL CAPACITY*

The applicant's Capacity Enhancement Agreement (CEA 05-006) allows up to 600 single family and 1,150 multi-family units. The applicant entered into a binding assignment agreement which gave them ownership of 600 single-family units and 596 multi-family units. This application creates a total development program for Phase 1 to 5 of the Randal Park PD of 574 detached single family units and 223 townhome units. The proposal does not exceed the allocated amount. But if unit mix changes are contemplated, as part of the submittal of the final plat application for each phase, the applicant shall demonstrate that the cumulative number of single family and multi-family homes does not exceed the CEA allocation or the assignment allocation. If a plat for any phase causes the cumulative number of single family homes to exceed 600 units, the applicant shall obtain written confirmation from OCPS approving a conversion of multi-family units to single family units prior to approval of the final plat by City Council.

10. *LOT LAYOUT RECOMMENDATION*

Although not required, applicant should consider re-configuring the lot boundaries/widths of Lots 44, 62, 224 and 542 so streets do not terminate directly in the center of the lot. Re-alignment will help decrease headlamps from cars facing directly into the fronts of the houses.

Urban Design

1. *MODEL CENTER*

Prior to issuance of a building permit for the first model home the applicant shall submit and have approved an administrative master plan for the model home facility including the layout and surfaces of parking areas, landscape, hardscape, lighting and fencing plans.

2. *HOUSING MODELS*

Per the approved PD Ordinance item 1.9 before submitting for a first building permit for a particular housing model, builders should request appearance review from the City Planning Division to ensure that elevations are consistent with the PD Ordinance and final SPMP conditions of approval.

3. *HOUSING VARIETY*

Lot sizes and building designs should be varied to avoid a monotonous streetscape. The following standards apply to single family detached dwellings:

- a. If 100 or more units of the same building types are proposed, at least four distinct models with at least three alternate elevations for each model must be provided. If front-loaded lots are proposed, the models must include a mixture of projecting and non-projecting garages.
- b. If less than 100 units of the same building type are proposed, at least three distinct models with at least three alternate elevations for each such model must be provided. If front-loaded lots are proposed, the models must include a mixture of projecting and non-projecting garages.
- c. Identical single family homes may not neighbor each other and may not face each other from across the street unless at least three of the following architectural elements are varied from each other: roof style; exterior color and material; architectural banding trim or cornice detail; window trim, the number of mullions or muntins, or shutters; a covered entryway or front porch design.

4. *STORMWATER PONDS*

The pond edges shall have littoral zone plantings along at least 33% of the edge in a band at least 6' in width. Plants shall be massed and planted to the appropriate submerged depths for the species. Plants shall be native to Central Florida and shall include species selected for both aesthetic and wildlife values. Recommended species include pickerelweed, spatterdock lily, fragrant water-lily, soft rush, and giant bulrush.

5. *PD ORDINANCE*

All conditions in the approved PD Ordinance No. 2010-48 for the Randal Park PD shall be applicable to all phases of the proposed development in this SPMP.

CONDITIONS OF APPROVAL

Transportation Planning

1. RESIDENTIAL PLANS SUBMITTAL

The submitted plot plan/site plan shall show the following: driveway approach, curb, street pavement edge, public sidewalk, etc. (Please see the Residential Plans Submittal Checklist: <http://www.cityoforlando.net/permits>). Reference Orlando Land Development Code (LDC), Chapters 60 and 65, and Engineering Standards Manual (ESM), Article Section 1.

2. HANDICAP RAMPS AT STREET INTERSECTIONS

Handicap (HC) ramps shall be constructed at the street intersection(s) and driveway connection(s) to comply with the Americans with Disability Act (ADA). Pedestrian ramps at street corners shall be designed to provide a separate ramp in each direction.

3. PEDESTRIAN CROSSINGS

The main pedestrian crossing areas between the buildings and the parking areas shall be clearly marked with pavement markings and maintained at all times in accordance with Manual of Uniform Traffic Control Devices (MUTCD) standards.

4. TRAFFIC ANALYSIS: MISC

Traffic Signal: Based on the results of the previous Traffic Signal Warrant Study, the owner / applicant shall be responsible for the design, materials and complete installation of the traffic signal at Dowden Rd. and Randal Park Blvd, and additional traffic signals as warranted, with the need determined at the sole discretion of the City Transportation Engineer.

5. TRAFFIC CONTROL DEVICES

Traffic control devices, in accordance with the manual on traffic control and safe practices, shall be installed and approved by the Traffic Engineering inspector prior to starting work. Responsibility for the installation of adequate safety devices for reducing the hazards to the traveling public, pedestrians and workers, as well as for safeguarding the work in general, shall remain with the contractor, utility company or other public agency.

6. DRIVEWAYS: SIGHT DISTANCE

Driveways - Sight Distance Requirements: The applicant shall design the site plan as necessary to comply with the City's Engineering Standards Manual street corner visibility requirements.

7. PUBLIC TRANSPORTATION FACILITY

Public Transportation Facility: The owner/applicant shall cooperate with the City and other applicable agencies to accommodate any reasonable future public transit service to the subject property.

INFORMATIONAL COMMENTS

Transportation Engineering

1. SHEET NUMBER C005

- The 5' rear setback for rear loaded lots shall not be permitted with the existing alley cross section of 12'.
- The roundabout shall be designed w/splitter islands on the approaches to act as pedestrian refuges. Refer to the FHWA Roundabout Design Guide.

2. SHEET NUMBER C006

- The intersection of "Street I" at "Street H" near the school site shall align with the school driveway.
- The alley for lots 122-131 shall be one-way SB to prevent vehicles from entering the alley so close to Randal Park Blvd.

3. SHEET NUMBER C007

Alleys intersections with local streets shall be as perpendicular to the curb as possible. This is especially the case where the alley intersects 90 degree curves.

4. SHEET NUMBER C008

- There is no need for a mid-block crossing on "Street Q" at lot 563.
- According to the PD, Randall Park Blvd or "Street P" was to connect all the way to "Street O" by way of "Street U".
- The intersections of "Streets S & Q" at "Streets O & T" are too close to each other.

5. SHEET NUMBER C011

"Street A" shall be built with Typical Section A-Spine Road since it will connect out to the future Econlockhatchee Trail extension.

INFORMATIONAL COMMENTS

Transportation Engineering (cont.)

6. SHEET NUMBER C013

- The gutter pan shall not be included in the width of any traveled way.
- At the most, only 1' of the gutter pan shall be accounted for when measuring parking stall/lane widths. Do not measure from the face of the curb.
- Marked parking lanes or parking stalls shall be at least 8' wide.
- Are the parking lanes on Typical Sections B, D, E, & F to be marked? If not, will parking be allowed on both sides? If not, "No Parking" signs shall be installed by the developer according to City Sign Shop specifications.
- Is parking allowed on Typical Sections C & G? If not, "No Parking" signs shall be installed by the developer according to City Sign Shop specifications.
- Any roadway with an asphalt width less than 24' shall have parking restricted on at least one side through the use of "No Parking" signs.
- What is the clear zone between the trail and the drop-off in Typical Section J?
- Since the alleys have center drainage, consider the use of a center concrete gutter.

7. SHEET NUMBER LA01-04

- Sight triangles for stop controlled approaches shall be measured according to City Code Section 60.142.
- Per City Code Section 60.230.a, street trees shall be spaced at least 50' apart.
- Street trees in the sight triangles shall have a canopy no lower than 8.5' above the pavement.
- Street trees shall be located away from street lights to prevent night time shadows.
- The sight triangles shall remain clear of bushes except for plant species less than 2' high.

8. ROAD TERMINATION

- Where a roadway stub to a future development is shown, the shall have a temporary termination that maintains driveway access to all lots and preserves the road right-of-way for the future connection.
- Any road terminations shall be designed according to City Code and the Engineering Standards Manual.
- In the event that a road termination cannot be constructed within the typical section right-of-way, an easement shall be provided by Randal Park for the road termination until the future connection is completed.

Transportation Impact Fees

1. TRANSPORTATION IMPACT FEES

Any new construction, change in use, addition, or redevelopment of a site or structure shall be subject to a review for Transportation Impact Fees. An estimated Transportation Impact Fee in the amount of \$1,985,218.00, based on the construction of 377 single family residential units and 216 multi-family residential units, will be due at the time of building permit issuance, subject to change upon final permit plan review.

For a copy of the complete ordinance or impact fee rate chart, you may reference our website at: <http://www.cityoforlando.net/planning/Transportation/ifees.htm>

Any exemptions or credits against the Transportation Impact Fee must be reviewed prior to permit issuance. All Transportation Impact Fee Credits shall be initiated and processed by the Transportation Impact Fee Coordinator.

Credit shall be available for the previous use located on the subject site. The Randal Park property has a remaining balance of \$3,207,365.00 in associated Transportation Impact Fee Credits. Note: the proposed Planning Board Case shall not affect the current dollar value or the credits, nor does it modify the Prior Rate Application date of December 31, 2013.

2. TRANSPORTATION CONCURRENCY

All new construction, changes in use, additions or redevelopments are required to submit a Concurrency Management application as a part of the building plan review process. A Concurrency Management application is available on the City's website: <http://www.cityoforlando.net/permits/forms/concurrency.htm>

The applicant shall comply with all applicable requirements of Chapter 59, the Concurrency Management Ordinance, to ensure that all public facilities and services are available concurrent with the proposed development. Approval of this application shall not be deemed to provide any vested rights.

INFORMATIONAL COMMENTS

Engineering/Zoning

1. CONCURRENCY

All new construction, change in use, additions, or redevelopments are required to submit a Concurrency Management application as a part of the building plan review process.

2. ENGINEERING STANDARDS MANUAL

The City Council Adopted the Engineering Standards Manual (ESM), Fourth Edition on March 9, 2009. All plans must conform to the ESM and all construction must be accomplished in accordance to the ESM.

3. SEWER-FDEP

This project requires a Florida Department of Environmental Protection (FDEP) permit for the sanitary sewer system. The Office of Permitting Services processes the permit for projects with reserved sewer capacity. At the time of FDEP permit submittal to the Office of Permitting Services, the following is required:

1. Permit Application - signed/sealed by the owner. This Office will complete page 10 of 11 when the construction plans are approved.
2. Construction Plans - six sets, signed/sealed by the engineer. The plans are to include the on-site and off-site sewer design together with the City's details. If a lift station is part of the sewer design, the engineer is to submit the shop drawings for the lift station (private or public).

The construction plans are reviewed by the City of Orlando's Waste Water Bureau and returned to the Office of Permitting Services when approved. This Office will contact the engineer to pick up the application and two sets of the approved plans or the transport to FDEP. The remaining sets will be retained by the Office for distribution.

Reminder: PLEASE ALLOW 3-4 WEEKS FOR THIS PROCESS

4. SEWER-LATERALS

All sanitary sewer construction is to be in accordance with the Engineering Standards Manual. The sanitary sewer lines are to be dedicated to and maintained by the City of Orlando. One lateral for each lot is required. Section 9.03.02 (J) of the ESM provides that a double wye is not acceptable.

5. SEWER-MISC.

Wastewater Comments: The following summarize the Wastewater Division comments relating to a revised roadway network and lot layout for Phases 2-5 of the Randal Park residential development:

- The gravity sewer layout to the sanitary sewer lift stations (LS-1 and LS-2) shall result in the same distribution of sanitary sewer flow as depicted on the Master Wastewater Plan and sanitary sewer calculations prepared by VHB for the Master plan dated Oct 2012.
- The connection to future development to the west shall not connect to the gravity sanitary and lift station system of Randal Park unless calculations are prepared that demonstrate the Randal Park gravity sanitary and lift station system have the capacity to serve additional flow from future development.
- The roadway and sewer in the southern most phase (currently known as Randal Park Phase 3), located between the proposed school and Phases 1A & 1B is under construction. Sanitary sewer main subouts to serve the proposed residential units in the southern most phase are incorporated into the Phase 3 construction plans. Individual laterals will not be allowed to be cut in to the newly constructed sewer. Sanitary sewer service to the southern most residential units shall be coordinated with the infrastructure currently under construction known as Randal Park Phase 3, case no. ENG2012-00937.

6. STORM-NPDES

Construction activities including clearing, grading and excavating activities shall obtain an Environmental Protection Agency (EPA) National Pollution Discharge Elimination System (NPDES) permit, except: Operations that result in the disturbance of one acre total land area which are not part of a larger common plan of development or sale.

7. STORM-FEMA

In accordance with Federal Emergency Management Act (FEMA) requirements, a revised letter of map revision may be required by the owner or engineer.

8. STORM-UNDERDRAINS

Underdrain construction is required in accordance with Section 17.01 of the Engineering Standards Manual.

INFORMATIONAL COMMENTS

Engineering/Zoning (cont.)

9. STORM-WATER MANAGEMENT DISTRICT

The owner/developer is required to design and construct an on-site storm water system in accordance with the Orlando Urban Storm Water Management Manual and the approved Master Drainage Plan. Approval from South Florida Water Management District is required. The system is to be privately owned and maintained.

10. STREET NAME

The proposed name of the new street shall be submitted to the Engineering Bureau for review for duplication of established street names, same sounding name, type of spelling, etc., in accordance with the City Land Development Code, Sections 58.64c34 (b) and 58.3122.

11. STORM-WETLAND

If the proposed wetlands are to be altered, the owner/developer needs to submit documentation from South Florida Water Management District and/or the Army Corp of Engineers.

12. PLAT

This property is required to plat in accordance with Section 65.401 of the City's Land Development Code prior to the issuance of building permits.

13. FLOODPLAIN

This site is located within a floodplain. The finished floor elevation must be one (1) foot above the 100' flood elevation.

14. STREET TREE FUND

The Orlando City Council approved a Resolution at the January 27, 1997 City Council Meeting. Section 61.226 of the City's Land Development Code provides for a Street Tree Trust Fund. The developer is required to contribute funds to the Trust Fund prior to issuance of the building permit. The City is responsible for installation of the trees. The cost is \$350 for each 12'-14' height of canopy tree. The developer may install the street trees in lieu of contribution to the Trust Fund in accordance with Section 61.226.

15. PLAT-MAJOR

Final Plat: At the time of final plat submittal, the following is required:

1. Mylar plat (executed by the owner and signed and sealed by the surveyor).
2. Certificate of Title or Title Opinion (A title insurance policy is not acceptable.) Refer to Florida Statutes 177.041 for additional information.
3. Initial 2 percent inspection fee based on estimated cost of the improvements. An itemized cost estimate is to be submitted to this office for approval.
4. Six sets of approvable construction plans.
5. Performance Bond - 110 percent of the cost of the improvements. The form is available in Engineering.
6. Joinder and Consent to Plat - If there is a mortgage on the property, a joinder and consent to plat document is required from each mortgage holder. The document is recorded with the plat. Refer to Florida Statute 177.081 for additional information.
7. Fixed Asset Report - The form is available in Engineering.
8. The plat must be reviewed by Orange County prior to recording. Begin this process by taking a copy of the executed plat to Room 1700 (Orange County Property Appraiser's Office, Attn. Rocco Campanale), 200 S. Orange Avenue, Sun Trust Bank Center. A copy of the completed Statement of Lien from Orange County is required by this office. The original Statement of Lien must be delivered to this office prior to recording the plat.

Police

1. CPTED REVIEW

The Orlando Police Department has received the plans for the Randal Park Phases 2-5 located at the NW corner of Dowden Rd and SR417. For new development in the City, OPD conducts a review of the various development plans utilizing CPTED (Crime Prevention Through Environmental Design) principles. CPTED emphasizes the proper design and effective use of a created environment to reduce crime and enhance the quality of life. There are four overlapping strategies in CPTED that apply to any development: Natural Surveillance, Natural Access Control, Territorial Reinforcement and Target Hardening. Since elevations and a lighting plan were not available at the time of the review, some recommendations are based on observations and recommendations from similar projects.

INFORMATIONAL COMMENTS

Police (cont.)

2. NATURAL ACCESS CONTROL

Design the site to decrease crime opportunity by denying access to crime targets and creating in offenders, a perception of risk.

- This can be attained by designing structural elements such as streets, sidewalks, building entrances and neighborhood gateways to clearly indicate public routes and discourage access to private areas.
- Examples include front porches to create a private space for the residents; balcony railings should never be made of a solid opaque material or more than 42 inches high; walkways and landscaping designed to direct visitors to the proper entrance and away from private areas.

3. NATURAL SURVEILLANCE

Design the site to keep intruders easily observable. This is promoted by features that maximize visibility of people, parking areas and building entrances; doors and windows that look out onto streets and parking areas; pedestrian-friendly sidewalks and street; front porches and adequate nighttime lighting. The following recommendations are based similar projects:

- The design of the single family and townhomes should provide residents with good Natural Surveillance from all sides of the homes.
 - All doorways that open to the outside should be well lit and front doors should be visible from the street.
 - Windows should be included on all sides of a house to provide full visibility of the property. Homes should be positioned so that each walkway between homes has Natural Surveillance, i.e., windows, from at least one of the homes. A fence without a gate should be installed if a walkway is bordered by a "windowless" side of both homes.
 - The driveway should be visible from either the front or back door and at least one window. If a style of rear-parking via alleys is to be used, OPD strongly recommends the addition of motion-sensor lights to the exterior of each house or townhouse. In addition, residents should be encouraged to utilize the builders' optional security system if it is available.
- Lighting plays a very important role in CPTED. It is crucial that lighting sends the right messages to the public about the safe and appropriate use of space at different times of the day and night.
 - An overall lighting plan should be submitted and all lighting for this project shall meet the guidelines in Orange County Ordinance No. 2003-08 and Orlando Land Development Code.
 - In order to give residents a sense of safety, alleys should not have any dark spots that can serve as hiding places.
 - Street lights should be included in the alleys and each garage should have lighting that is controlled by the resident.
 - Appropriate lighting should be included in all common areas throughout the project if it is anticipated that these areas will be utilized after-dark.
 - If a style of rear-parking via alleys is used, street lights should be included in the alleys.
 - Pedestrian-scale street lighting should be used in high-pedestrian traffic areas.
- Landscaping is another crucial aspect of CPTED. Trees branches should be kept trimmed to no lower than six feet from the ground and shrubs should be kept trimmed to no higher than 30 inches. Avoid conflicts between landscaping and lighting, especially lighting adjacent to canopy trees. Landscaping should not create blind spots or hiding places. Open green spaces and community recreational areas should be located so that they can be observed from nearby homes.
- Benches placed throughout the common areas and park tracts are a good way to increase surveillance. However, consider bench designs that encourage stopping and resting but reduce opportunities for illegitimate users (i.e. a ribbed design rather than solid and center rails or arm rests to discourage sleeping).

4. TERRITORIAL REINFORCEMENT

Territorial Reinforcement: Design can create or extend a sphere of influence, where users develop a sense of territorial control, while potential offenders are discouraged.

- This is promoted by incorporating features that define property lines and distinguish private spaces from public spaces such as; landscape plantings, pavement designs, gateway treatments and CPTED post and pillar fences.
- Lots, streets and houses should be designed to encourage interaction between neighbors.
- Each house address should be clearly visible from the street with numbers a minimum of five-inches high made of non-reflective material.

INFORMATIONAL COMMENTS

Police (cont.)

5. *TARGET HARDENING*

This can be accomplished by features that prohibit entry or access such as window locks, dead bolts for doors and interior door hinges.

- Exterior doors should be hinged on the inside and should have a single cylinder deadbolt lock with a minimum one-inch throw.
- Interior doors that connect a garage to the house should have a single cylinder deadbolt.
- Door locks should be located a minimum of 40 inches from adjacent windows.
- Homes should not have jalousie, casement or awning-style windows. All windows should have locks.
- Sliding glass doors should have one permanent door on the outside and the inside moving door should have a docking device and a pin.
- In addition, the developer should join with the residents to create a Neighborhood Watch. Neighborhood Watch training and other residential crime prevention strategies are available from OPD; please contact our Neighborhood Watch Specialists at 407-246-3628 or 3773.

6. *CONSTRUCTION SITE PROTECTION*

Due to the threat of theft of building materials and equipment from construction sites, Orlando Police Department's Crime Prevention Unit strongly recommends the developer institute the following crime prevention/security measures at this project site:

- Post signs at the site that theft from the site or trespassing on a construction site is a felony under Florida Law and that the developer will prosecute.
 - So that OPD patrol officers can see possible offenders, perimeter lighting should be installed at a minimum of 150 foot intervals and at a height not less than fifteen (15') from the ground. The light source used should have a minimum light output of 2,000 lumens, shall be protected by a vandal resistant cover, and shall be lighted during the hours of darkness.
 - In addition to lighting, one of the following physical security measures should be installed:
 - Fencing, not less than six (6') feet in height, which is designed to preclude human intrusion, should be installed along the perimeter boundaries of the site and should be secured with chain and fire department padlocks for emergency vehicle access; post in a clear area; or
 - A uniformed security guard should be hired to continually patrol the construction site during the hours when construction work has ceased.
 - Valuable construction materials and tools should be protected in a secondary fenced, locked cage.
 - Post in a clean, open area, the name and numbers of emergency contact persons for OPD in case of a night-time emergency.
- If you have any questions, please call the Crime Prevention Unit, Officer Edgar Malave, at 407-246-2461.

Fire

1. *DEPT ACCESS*

Every building constructed shall be accessible to fire department apparatus by way of access roadways with all-weather surface of not less than 20 feet of unobstructed width, adequate roadway turning radius, capable of supporting the imposed loads of fire apparatus, and having a minimum vertical clearance of 13 ft. 6 in. [NFPA 1: 18.2.3]

2. *DEPT ACCESS DEAD END*

A dead-end in the fire department access road in excess of 150' will require an approved turnaround or cul-de-sac. [NFPA 1: 18.2.3.4.4]

3. *FLORIDA FIRE PREVENTION CODE*

Be advised that any new construction must adhere to the requirements of the Florida Fire Prevention Code, 2010 Edition, and The City of Orlando Fire Prevention Code.

4. *FIRE CODE REVIEW*

TRC fire code review is preliminary in nature, and is intended to expose or prevent evident design deficiencies with State and City Fire Codes. The design will be reviewed in detail for State and City Fire Code compliance at the time of permit application.

5. *ACCESS TO BUILDINGS*

A fire department access road shall extend within 50 ft of a single exterior door providing access to the interior of the building. Fire department access roads shall be provided such that any portion of the facility or any portion of an exterior wall of the first story of the building is located not more than 150 ft (450 ft for sprinkler protected structures) from fire department access roads. [NFPA 1: 18.2.3.2]

INFORMATIONAL COMMENTS

Fire (cont.)

6. *UG-FIRE PERMITS REQUIRED*

Underground main contractor must apply for a Fire permit for the installation or modification of any underground mains serving fire hydrants and/or fire protection systems prior to any installation. If the water distribution system and fire hydrants are located in a right-of-way or recorded easement and owned/installed/maintained by the water purveyor, we will only require installation to the water purveyor's standards for underground components and connections. However, hydrants must be in compliance with hydrant spacing, location, distribution, color coding, and needed fire flow minimums as specified in City Fire Code. Underground main and fire hydrant installations on private property will require an FIR permit and full compliance with NFPA 24. [City Fire Code, Section 24.13(t)(13)]

7. *TRC-CONSTRUCTION PHASE*

Please inform contractor that where underground mains and hydrants are to be provided, they shall be installed, completed, and in service prior to construction work. [NFPA 1: 16.4.3.1]

Fire department access shall be provided at the start of the project and shall be maintained throughout construction. [NFPA 1: 18.2.2.1]

In all buildings more than one story in height, at least one stairway shall be provided that is in usable condition at all times and that meets the requirements of 7.2.2 of the Life Safety Code, NFPA 101. [NFPA 1: 10.2]

8. *REQUIRED FIRE FLOW*

All structures must be protected by fire hydrants in accordance with City Fire Code 24.30(f). A determination will be made at the time plans are submitted for permitting. We will need a Required Fire Flow calculation in accordance with the ISO method demonstrating the water distribution system and new/existing fire hydrant(s) can deliver the demand.

Parks

Prior to any construction or grading, a tree removal/encroachment permit must be obtained.

CONTACT INFORMATION

City Planning

For questions regarding City Planning review, please contact Colandra Jones at 407.246.3415 or colandra.jones@cityoforlando.net.

Urban Design

For questions regarding Urban Design Review, please contact Doug Metzger at 407.246.3414 or douglas.metzger@cityoforlando.net.

Transportation Planning/Transportation Impact Fees

For questions regarding Transportation Planning plan review, please contact Nancy Ottini at 407-246-3526 or nancy.jurus-ottini@cityoforlando.net

Transportation Engineering

For questions regarding Transportation Planning plan review, please contact Jeremy Crowe at 407-246-3262 or jeremy.crowe@cityoforlando.net

Engineering/Zoning

For questions regarding Engineering or Zoning contact Keith Grayson at 407.246.3234 or keith.grayson@cityoforlando.net.

Police

For questions regarding the Orlando Police Department plan review, please contact Audra Nordaby at 407-246-2454 or audra.nordaby@cityoforlando.net.

Fire

For any questions regarding fire issues, please contact Jack Richardson at 407.246.3150 or at jack.richardson@cityoforlando.net. To obtain plan review status, schedule/cancel an inspection and obtain inspection results, please call PROMPT, our Interactive Voice Response System at 407.246.4444.

Parks

For questions regarding a tree removal/encroachment permit, please contact Justin Garber at (407) 246-4047 or justin.garber@cityoforlando.net.

REVIEW/APPROVAL PROCESS—NEXT STEPS

1. City Council approves the MPB minutes and the Specific Parcel Master Plan becomes effective.
2. Prior to the issuance of building permits, the project is subject to an appearance review to ensure consistency with the Randal Park PD.